

2025 Season

Rules & Regulations

Saturday Divisions

- Late Model
- Street Stock
- Sport 4's
- Cage Runners
- Tigers
- Limited Sportsman

Wednesday Divisions

- Modified Enduro
- Full Size Trucks
- Stars of Tomorrow
- Road Runner



www.speedway95racing.com

Revised December 1,

SPEEDWAY 95 GENERAL RULES	2
TRACK SAFETY	2
SAFETY RULES	3
MINORS / PETS.....	6
GENERAL RULES	6
GENERAL RACE RULES	8
POINTS	11
NUMBER RULES	11
PENALTIES	11
TECH DEPARTMENT.....	12
TEARDOWNS	13
FLAG RULES	14
LINE-UP PROCEDURES.....	15
TRANSPONDERS	15
TRANSPONDERS	16
LATE MODEL RULES.....	17
LIMITED SPORTSMAN	22
STREET RULES	29
SPORT FOUR RULES	33
TIGER RULES.....	38
MODIFIED ENDURO RULES	45
ROADRUNNER RULES.....	48
TRUCK RULES	52
RACING STARS OF TOMORROW RULES	54
ROLL CAGES	56

SPEEDWAY 95 GENERAL RULES

- “Race Meet” shall, for the purpose of Rules enforcement, be considered to be the period of time between the opening and closing of the Pit Entrance Gate.
- “Race Event” shall, for the purpose of Rules enforcement, be considered to be the period of time between the opening and closing of the track surface.

INTRODUCTION:

- The rules, regulations and specifications set forth in the SPEEDWAY 95 rule book have been compiled around careful consideration of guidelines that will best ensure safe, competitive, and affordable race events.
- At any time, should it become necessary in the interest of fair competition, economy, race promotion, or in the interest of the race show itself, the SPEEDWAY 95 Management reserves the right to modify, add or delete any rule, regulation, or specification.
- The Race Director shall be empowered to permit minor deviation from any of the specifications herein or impose any further restrictions that, in his/her opinion, do not alter the minimum acceptable requirements. No express or implied warranty of safety shall result from such alteration of specifications. Any interpretation or deviation of these rules is left to the discretion of the officials. Speedway 95 Officials decision is final.

DISCLAIMER:

- The rules and/or regulations and specifications set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events.
- These rules will govern all SPEEDWAY 95 events, and by participating in these events all competitors and their associates are required to comply with these rules.
- No express or implied warranty of safety shall result from publication of, or compliance with, these rules, regulations, and specifications.
- The rules within this book are intended as a guide for the conduct of the sport and are in no way any guarantee against injury or death to participants.

ENFORCEMENT:

- Since it would be both impractical and nearly impossible to list within the confines of any rule book, all the modifications, adaptations and infractions that could and would be illegal, it should be understood for the purpose of rule enforcement that only those items that are specified in this book are allowable and permissible.
- In addition, Speedway 95 management holds the right to suspend, eject, and/or withhold purse and points from anyone who violates these rules.

TRACK SAFETY

1. Drivers are permitted to inspect the racing surface and racetrack area to learn of any defects, obstructions, or anything which, in their opinion is unsafe and they shall report concerns to the management of Speedway 95.

2. Any driver entering a racing event is considered to have inspected the track and to have determined that all conditions are satisfactory to them. If they find the conditions unsatisfactory, **THEY SHOULD NOT RACE**. This further indicates that they are aware that auto racing involves risks, and they assume these risks with full awareness and knowledge. SPEEDWAY 95 assumes no responsibility for damage to or loss of your equipment, vehicle or any parts by any means whatsoever.
3. It is the goal of the staff and management of SPEEDWAY 95 to provide motorsports competition in as reasonably safe a facility and conditions as possible. If in the opinion and judgment of the track management, any individual or individuals presents a harm or danger to anyone, the management maintains the right, at any time, to suspend, restrict, fine, or ban, any individual, including any driver, owner, team or team member, or any person, in the interest and in consideration of providing safe racing conditions for the majority of competitors, spectators, and staff.
4. If a person is involved in an accident involving a personal injury, they should inform track officials / management immediately so that the necessary medical care can be provided, and insurance reporting will be accomplished. No claims will be considered unless reported prior to leaving the SPEEDWAY 95 property that day. Any person(s) denying possible injury, opts not seek and/or refuses evaluation by track medical team forfeits any and all insurance protection.
5. Firearms are strictly prohibited on SPEEDWAY 95 property. Any person discovered having firearms will be immediately suspended for the remainder of the race meet and/or expelled from the property for a period of time as determined by SPEEDWAY 95 Management.
6. Alcoholic beverages and illegal, non-prescribed drugs or substances are prohibited from SPEEDWAY 95 property or in any area that is publicly used for the operation of speedway facilities. Illegal drugs are as defined by state and/or federal law. Non-prescribed drugs are those not authorized by a state-licensed pharmacist. Any person(s) found to be in possession of and/or under the influence of alcohol and/or any illegal, non-prescribed drug may be immediately expelled from the property. Prescribed drugs being used by competitor(s) must be reported by that person(s) to SPEEDWAY 95 management prior to entering any aspect of competition.
7. Any person(s) considered causing a disturbance of any kind (verbal or physical), disrupting the orderly conduct of a race event, threatening the safety and well-being of others (staff or patrons) will be immediately suspended for the remainder of the race meet and/or expelled from the property for a period of time as determined by SPEEDWAY 95 Management.
8. Motorsports at all levels pose a risk to your wellbeing. Following the rules set forth by SPEEDWAY 95 is no way a guarantee or suggests your safety.

SAFETY RULES

1. **BATTERIES** (RoadRunners are Excluded)
 - a. Battery must be mounted within the car and must be securely bolted to the floor.
 - b. Must be mounted in a battery box.
 - c. Must be covered to prevent spillage.
 - d. Batteries and Fuel can not be mounted in the same compartment.
2. **FIRE EXTINGUISHER**
 - a. All cars in all divisions must carry an operative 3# minimum fire extinguisher with gauge in working charged status.
 - b. Must be within reach of the driver while belted into driver seat.
 - c. All extinguishers must have a metal quick release mounting bracket with a seatbelt type fastener.

- d. NO plastic brackets, tape, wire, or other makeshift devices will be allowed.
- 3. WINDOW NETS
 - a. Window Nets are required for all divisions.
 - b. Must be fastened to the roll cage and must be quick release with a seat belt type fastener.
 - c. Window nets must be securely fastened at all times when a car is on the racing surface.
- 4. HELMETS
 - a. All helmets must be of racing or motorcycle type and must be Snell approved.
 - b. Helmets must be always worn when a car is on the racing surface.
- 5. DRIVE SHAFT
 - a. A drive shaft safety loop or under sling is required within 12" of the front universal joints.
 - b. The drive shaft must be painted white.
- 6. BRAKES
 - a. Four-wheel brakes in proper working order are mandatory at all times.
 - b. Brake shoes or pads must be mounted and operating on all four wheels (except Late Model division cars).
- 7. OVERFLOW CAN
 - a. A four (4) quart minimum overflow can, connected by hose to the radiator and mounted in the engine compartment is mandatory.
- 8. BUMPERS
 - a. Bumpers must be welded to the frame for safety and must be stock in appearance.
 - b. NO BATTERING RAMS.
 - c. Rear bumper/bumper covers strapped to body or frame.
- 9. DOORS
 - a. All doors must be welded shut.
- 10. FENDERS
 - a. Fenders may be trimmed for tire clearance only.
 - b. All trimmed edges must be rolled to eliminate rough edges.
- 11. HOOD
 - a. Hood and trunk must be securely fastened down at all times.
 - b. Enforcement of this rule is at the discretion of the flagman/race director, and his decision is final.
- 12. FIREWALL
 - a. All cars must have a steel firewall of at least .22-gauge thickness between the engine compartment and the passenger compartment.
 - b. as well as between the passenger compartment and the trunk area containing the fuel cell.
- 13. FUEL CELL
 - a. All cars must have a fuel cell with foam inside mounted in a box of no less than .22-gauge steel.
 - b. All fuel cells must be securely fastened to the vehicle in the trunk area and have a minimum of 8" ground clearance.
 - c. Fuel cells must have a vent hose with check valve. Vent hose must extend below fuel cell when car is in the upright position.
- 14. GLASS
 - a. A full windshield of either safety glass or 1/8" Lexan™ is required in all divisions. If a windshield is lost during a race meet, the driver may continue to race with approval of the Race Director if

he/she wears goggles or a full-face shield and installs at least 1/4" mesh screen over the windshield area.

- b. Cracked or broken windshields must be cleared by the Safety Committee before a car will be allowed to compete.

15. STEERING COLUMN

- a. Steering columns must be collapsible or have at least two (2) universal joints. Steering box "rag" joint and heim joints supporting steering column are not considered universal joints.
- b. Center of steering wheel must be padded.
- c. All divisions must have quick disconnect steering wheels.

16. ROLL CAGE (RoadRunners are Excluded)

- a. A full roll cage of four uprights joined at the top by a complete loop is required for all divisions.
- b. Front hoop and supporting bracing is mandatory.
- c. Front hoop is optional in Street Division.
- d. All piping must be at least 1 1/2" O.D. steel and welded 360 degrees.
- e. No galvanized pipe allowed.
- f. No threaded fittings or flattened ends will be allowed.
- g. Three (3) door bars on the driver's side are mandatory, with proper bracing and gussets. Two (2) door bars with proper bracing and gussets are required on the "Passenger" side, with three strongly recommended. These bars may either follow the contour of the door or be square with front and rear uprights.
- h. A complete hoop is required overhead with at least one bar either diagonally across or front to rear. At least two braces must extend behind the rear up rights to a point on the frame at least three feet back. Bracing extending into the trunk area to protect the fuel cell is recommended.
- i. A horizontal bar between the two front uprights in the dashboard area is recommended.
- j. A driver side vertical "vent window" brace is recommended. A plate of at least 1/8" thickness is required to welded and gusseted between the door bars and the body panel on the driver's side. The plate should be located so as to protect the driver from foreign objects that may pierce body panel.

17. SEATS AND SEAT BELTS

- a. aluminum seats are required in all divisions. (RoadRunners are Excluded)
- b. Seats must be mounted attached to the roll cage.
- c. Seat belts must be airplane type quick release and capable of withstanding 3,000# body block test.
- d. The seat belts must be attached at two separate points on the roll cage equal distance from the center of the seat back and no less than twelve (12) inches apart. They must be attached with bolts having a minimum diameter of 1/2". Bolts must be aligned so that the direction of stress will be in direct line with the pull of the seat belt. If belts are attached to a steel plate attached to the frame, the plate must be a minimum of 1/4" thick.
- e. Shoulder straps are mandatory and must be securely fastened to the roll cage behind the driver's seat or to a 1/4" plate welded to the roll cage behind the driver's seat.
- f. Sternum and submarine (crotch) belts are highly recommended.
- g. Seat belts and helmets must have a date of manufacture not more than four years old.
- h. 5-point safety harness required; harnesses must have a date of manufacture not more than six years old unless approved by Safety Team.

18. CLOTHING

- a. All drivers in all divisions must wear an approved long sleeve fire retardant racing suit and racing gloves at all times when the vehicle is on the racing surface.
 - b. This includes test and tune sessions.
 - c. Racing suits must be free of rips, tears and holes, and have a date of manufacture of not more than four years old, to be checked a few times a year by the safety committee.
19. Any jack points protruding through the body panels must be approved by the Safety Committee. Safety Committee approved 1" rub rails mounted flush with body panels allowed in all divisions.
 20. No boxing of the passenger side of the interior will be allowed if it prohibits the easy removal of the driver through the passenger side of the car.
 21. **All vehicles must have front and rear tow hooks made of 1/2" rolled steel and installed as a vertical loop. Late Model cars may make tow arrangements under the hood or trunk of the car.** The track and its tow crews will have no responsibility whatsoever for damage resulting from towing a disabled vehicle from the racing area.
 22. The Safety Committee has the right to disqualify any car from competition based on any infraction of the safety rules. Any infractions must be re-inspected before that car will be allowed to compete. The Safety Committee is responsible for the placement and visibility of numbers.
 23. No anti-freeze in cooling systems. Water and KEEP KOOL™ quick cool only.
 24. Any weight added to vehicles must be lead only and painted white and have the car number on it.

MINORS / PETS

1. There is no age limit in the pits at SPEEDWAY 95. Those under 18 years of age (hereafter referred to as "Minors"), must present a current and completed SPEEDWAY 95 Minor Release Form to enter the pits. We request that all minors 12 years old and under be accompanied by an adult while in the pit area.
2. Dogs are not permitted in the Pit Area or Grandstand Area of SPEEDWAY 95 during a race meet. The only exception is the use of a medically approved service dog, which must be reported to Speedway management upon entry.

GENERAL RULES

1. PIT AREA

- a. Everyone entering the Pit Area shall pay the required entry fee. The entry fee paid by a driver shall enter them for one race event; drivers wishing to enter for more than one race event shall pay an additional entry fee for each race to keep the payout system fair and consistent. Upon entry, drivers must additionally register for their race event at the middle pit admission window.
- b. Only vehicles towing race cars will be allowed to park in the pit area. All other vehicles may be allowed for the purpose of delivering equipment, people, etc. and then must return to the public parking area.
- c. The speed limit in the pit area is 5 mph for all vehicles. Always use caution and remain alert in the pit area. Race cars have the right of way in the pit area.
- d. Pit Pad rentals are not mandatory but recommended if you want to use the same pit stall each time you race. Reservation fee is \$100 for the season. If you choose not to rent a pit pad, then you will use whatever is open on the night you race, first come - first serve, no guarantees. Pit Pads are not reserved for Special Race Events, only regular Speedway 95 Events.

- e. Concrete pads in the pit area are the parking positions of race cars only. During special events, every attempt shall be made to locate visiting race teams on undesignated pads; however, should a mistake be made, the pad will remain the possession of the visiting team for that race meet. Should this be a concern to you, please arrive early for the event.
- f. Pit pads not in use on a given race meet may be assigned to any other team for use that day by track management.
- g. No race car shall carry passengers at any time while in the pit area or on the racing surface.
- h. No person(s) shall have any claim whatsoever for disqualifications, damages to the car and/or driver, expenses or otherwise against track officials, owners, operators or promoters. All entries through Pit Gate understand the potential hazards and risks and agree to self-responsibility.
- i. SPEEDWAY 95 will help with the disposal of race tires. We request tires to be disposed of piled at one corner of the pit pad prior to your departure.
- j. SPEEDWAY 95 will help with the disposal of trash and race car debris. We request race car debris to be piled at one corner of the pit pad and trash placed in the provided trash receptacle prior to your departure.
- k. SPEEDWAY 95 Management reserves the right to reject any entry for failure to comply with the Rules. Any individual(s) not complying with the Rules shall be penalized as set forth in these Rules or as determined by SPEEDWAY 95 Management.
- l. Any cars left at SPEEDWAY 95 for more than 7 days after the conclusion of the racing season will be removed at the car owner's expense.

2. DRIVER ELIGIBILITY / CONDUCT

- a. Each driver must be approved for competition within any division by track management. Decisions are based upon a driver's previous experience, as well as their past and projected performance and ability. All decisions are the opinion of track management and are solely administered by track management, without the right of appeal. All decisions are made in the best interest of fairness of competition, safety to the individual driver and all other drivers, pairing of drivers with similar experience and talent, and in the overall best interest of the positive enhancement of a division. Additionally, decisions are made in the interest of limiting damage and expensive repairs to all teams.
- b. Track management may at any time require a physical examination of a driver or a doctor's release form before they are allowed to compete. The judgement of the track EMT and/or track management is final and without recourse as to fitness.
- c. Any person deliberately going into another competitor's designated pits area in a violent or threatening manner shall be subject to expulsion and suspension. An altercation shall be defined as any vehement argument, quarrel, disagreement, dispute, exchange, squabble, clash, and/or hostile expression of a difference of opinion. Any driver, car owner, or crew member found guilty of such altercations by making contact with an official may be subject to suspension. The track management will weigh the circumstances and decide on the degree of the penalty before the next regularly scheduled race meet.
- d. After any suspension, expulsion, rejection, penalty or restriction from participation has been issued, SPEEDWAY 95 Management reserves the right to review the circumstances on a case-by-case basis and decide as to whether or not the person(s) involved can maintain the capabilities to resume participating at SPEEDWAY 95. This will be done in accordance with the current general rules, guidelines, and safety objectives, and in the opinion of SPEEDWAY 95 Management.

- e. The Driver will be the sole spokesman for his/her race team in any/all matters pertaining to events during the race meet. Additionally, the Driver shall bear responsibility for the conduct of his/her team owners, crew members, sponsors, spouse/family and/or associates in any/all matters during the race meet.
- f. Any driver (other than Stars of Tomorrow drivers) that is under the age of 18 must have a valid and legal learners permit or driver's license. Any minor that is interested in racing must be approved by SPEEDWAY 95 management.

GENERAL RACE RULES

- a. Raceceiver's and Transponders are required for all weekly competitors at SPEEDWAY 95.
 - i. The Raceceiver may be worn by the driver or spotter.
 - ii. Transponders shall be mounted outside of the car on 12'6" from the front bumper.
 - iii. Transponders are required on the car and working for all practices, heats, and features.
- b. All cars must take the position in the lineup as assigned by the Pit Tower (see Handicap/Lineup procedures). Drivers/Teams are required to notify Pit Tower of any/all changes, driver requests, etc. prior to the event. Note: If a competitor arrives at the track after all practice sessions have been completed, track management reserves the right to require the competitor to forfeit their assigned heat starting position and start the heat at the rear of the field.
- c. Should a race car become disabled the driver is permitted to compete with a borrowed car registered to race in the same division. The Pit Tower official must be notified prior to entering the race. The driver using a backup or borrowed car will forfeit their starting position and start at the rear of the field. Drivers may not switch cars after the race event has started. Should the borrowed car be determined illegal at the conclusion of the event, the driver will forfeit the position and purse.
- d. Once a race car is on the track, it is under the directive of the Flagman and/or Race Director. Obedience of all directions and/or decisions is required.
- e. Race penalties will be assessed at the sole discretion of the Race Director and/or Flagman. Decisions will be based on the observation of the incident with further assistance from on track officials. The penalties will be based on the severity of the incident and may include a warning, being parked for remainder of the race, disqualification from the race, suspension from further race events and any other penalty as determined by SPEEDWAY 95 management.
- f. Rough riding, excessive track use, intentionally causing race delays, and all unsportsmanlike conduct of any kind will not be tolerated. Purposely damaging a competitor's car is considered to be criminal behavior, will not be tolerated and may be subject to disqualification, suspension, up to and including prosecution.
- g. Drivers and teams determined to be guilty of the above behaviors, whether on or off the track will be immediately suspended for the remainder of the race meet and/or expelled from the property for a period of time as determined by SPEEDWAY 95 Management.
- h. There will be no caution/restart if a spinout or accident, in the Race Director and/or Flagman's opinion, the track is not blocked, the driver is not injured and there is no immediate danger. Drivers determined to purposely "sit & wait" creating an unnecessary caution/restart (Deliberate Caution) will align at the rear of the field and will be scored one lap down at the discretion of the Race Director and/or Flagman.

- i. All cars participating and/or involved in a caution/restart shall go to the rear of the line-up as assigned and directed. Two (2) car incidents may opt to invoke the Gentleman Rule as described below:
 - i. *GENTLEMAN RULE – In the event a competitor makes contact with another resulting in a caution, the common result/rule is for both cars to go to the rear of the field for the ensuing start. The Gentleman Rule enables a driver to voluntarily accept responsibility for the incident, thus allowing the other car involved to restart in their current position. This is intended for a driver that unintentionally hits the car in front of them or a driver that slows and unintentionally causes the car following them to make contact. To invoke the Gentleman Rule, the driver only needs to stop near the flag stand and tap his/her roof notifying the Flagman and/or Race Director. At that time, the driver will be the only car put to the rear for the restart. This rule is intended to introduce a level of sportsmanship that is welcome and sometimes lacking in racing. To avoid speculation and conflict, this rule will only be implemented in a two (2) car incident. Should three (3) or more cars be involved, then all cars are subject to go to the rear of the field at the discretion of the Race Director and/or Flagman for the ensuing start.*
- j. Pole car has choice of inside or outside pole on restarts in both heats and features after original lap is completed.
 - i. If the first lap is not completed, the line up will go back to a complete and original lineup.
 - ii. Pole drivers have the responsibility to set the pace for the start or restart. These drivers will not accelerate on the starts or restarts until their cars are in the starting box in corner 4.
 - iii. Any car deemed to have jumped a start will be docked by two positions per car passed at the next caution flag. If no caution flag is waved that the penalty will be assessed at the completion of the race.
- k. Any car pitting under caution must align at the end of the field safely under the direction of Track Officials.
- l. Track Officials shall dispatch appropriate equipment for cars requiring assistance and drivers shall not request specific equipment. For reasons of safety and assistance, drivers shall remain with their car. Should a driver exit the track leaving his/her car, it shall be considered abandoned. Track Officials will have the option to tow any car assisted by wrecker or flatbed, particularly those without required tow chains/hooks may be taken to the infield and remain until the end of the race allowing the race to resume as quickly as possible.
- m. Any car spinning by itself three (3) times in one race or involved in multiple cautions is subject to removal from that race. Any car that is significantly off the pace being set by the other cars in the race or that is being driven in an erratic fashion is similarly subject to being black flagged and parked for the remainder of the race.
- n. In the interest of safety, drivers involved in cautions due to a wreck or breakdown must stay in their car with safety equipment on until instructed by a Track Official and/or EMT that is safe to get out. THE ONLY EXCEPTION is if the driver feels he/she is in immediate or imminent danger – ie – fire. Once the driver has been allowed out of the car, they MUST remain in the immediate vicinity of the car or with a Track Official. Any driver failing to comply will be subject to a penalty (see penalty section)

- o. Drivers are not permitted to use the backstretch pit access (track entrance) to leave the track (unless specifically instructed by a track official). Any driver failing to comply will be subject to a penalty (see penalty section)
- p. At the discretion of the Race Director and/or Flagman, at any time, a car not in competition with and considered hazardous and/or considered to have a safety issue either to themselves or other competitors; may be sent to the pit area. All repairs must be made in the pit area and not on or on the side of the track surface. If after making necessary repairs, the car is able to compete, the car may return to the race at the end of the field.
- q. Race Director / Flagman may order single-file restarts if sufficient progress to a conclusion of any race is not being made.
- r. Race Director / Flagman reserves the right to conclude any feature race after 30 minutes if sufficient progress is not being made toward the conclusion of the race.
- s. The Driver shall be the sole spokesperson for their race team in any and all matters pertaining to events during the race meet (includes owners, crew members, sponsors, spouse/family and/or associates of a driver as important members of the race team). Additionally, the Driver shall bear responsibility for the conduct of his/her team owners, crew members, sponsors, spouse/family and/or associates in any/all matters during the race meet.
- t. A feature race shortened due to weather or unusual conditions will be considered complete if fifty percent (50%) or more of the scheduled laps have been completed. The last completed green flag lap will be used to determine the finishing position.
- u. A feature race that must be stopped or “red flagged” during the final five (5) laps of competition may be considered complete at the discretion of the SPEEDWAY 95 Management. The final finishing position will be determined from the last lap raced and scored under the green flag with car(s) causing the stopping of the race removed from the last green flag lap and scored using usual scoring procedures
- v. If there is a yellow or red flag after the race leader has been given the white flag (signaling the final lap), then the field is frozen, and the race declared complete. There is no racing back to the checkered flag. The official finish order of the race will be the running order of last completed green flag lap minus any cars deemed to be involved in the caution.
- w. A race meet shortened due to weather or unusual conditions shall be considered “official” if all heat races have been completed. Earned points will be awarded accordingly. Purse money will be paid for completed features only. Any features not run may be rescheduled to the next available by track management.
- x. If a division’s scheduled heat AND feature races are lost to weather, they will NOT be made up. In the event that a division’s heat races have been completed but the feature is rained out, SPEEDWAY 95 Management will make every effort to reschedule the makeup feature for a future date. The makeup feature will be lined up based on heat race results. SPEEDWAY 95 Management reserves the right to allow non-qualified competitors to pay entry fee and race in rescheduled race (lined up at rear of field)
- y. Any incident not covered by these rules may be decided upon by SPEEDWAY 95 Management and may be made a rule for the remainder of the season.
- z. Pit tower attendant must be notified of any change in drivers during a race meet and must be notified of change prior to the race.

- i. If this is not done prior to running the race, the driver/owner shall forfeit all purse, points, and position for race which the offense took place.
- aa. The same car may not be driven in more than one division during the same race meet.
- bb. Drivers and track personnel shall not attempt to fix any car on the racing surface.
- cc. During all caution lineups all lapped cars in the feature will go the rear of the field inside 10 laps to go.
- dd. No Cell Phones of any kind allowed in the racecars at anytime while on the race surface. Any driver caught using a cell phone will be disqualified from the race meet.

POINTS

Feature Winners in all divisions are awarded 100 Points.

- i. Each Position back is awarded two (2) less points than the previous position.

Heat Winners in all divisions are awarded 10 Points.

- ii. Each Position back is awarded two (2) less points than the previous position.
- iii. 6th place finisher in Heat races will receive one (1) point.

To receive any points for either a heat or feature race drivers must cross the start/finish line and take the initial green flag and complete a full lap under green flag conditions.

Points are awarded to the driver. Purse Money is paid to the person listed on the W9 for the car.

Any other arrangements are between that person and the driver.

In case of tie finishes where a clear and convincing winner cannot be determined, the purse and points for both or all positions in question will be pooled and divided amongst the drivers involved.

ANY DISCREPANCY IN POINTS OR FINISHED MUST BE ADDRESSED WITHIN 2 WEEKS OF EVENT.

Competitor must compete in minimum of $\frac{3}{4}$ of the scheduled race events and purchase a ticket to the awards banquet to qualify for a trophies.

NUMBER RULES

All Drivers must register on the track website using my racepass. Numbers are not given / assigned until registration form is completed online at www.myracepass.com/tracks/2594/registrations Number will be no more than 2 Digits and one letter. Three-digit numbers and fractions are not allowed.

All numbers must be kept in easily readable condition. If they are damaged in a accident, they must be fixed by the next race meet.

All Numbers must be a contrasting color and easily readable for the scoring tower.

Inability of scoring to read numbers can result in the car being barred from competition until they are repaired to the satisfaction of the race director.

PENALTIES

- a. JUMPING START (failure to stay in line until start/finish line) 1st offense = 2 pos per car passed penalty on next flag 2nd offense = 1 lap penalty on next flag 3rd offense = parked & scored last
- b. Leaving the vicinity of the car / walking off the track (without permission from track official or medical staff) or to confront or gesture to another driver = parked for remainder of race & scored

last place. If driver leaves car unattended on the track, the car will be moved to infield and released at next available opportunity or end of race meet

- c. Use of backstretch track entrance to exit track (without permission from track official) = parked for remainder of race & scored last place
- d. Exiting track or un-approved movement under Red Flag (without permission from track official) = parked for remainder of race & scored last place
- e. “Deliberate” Caution = 1 lap penalty
- f. Deliberately hitting another competitor’s car under caution or following completion of race = full event disqualification and possibility of suspension.
- g. Verbal or physical abuse of a track official = full event disqualification and possibility of suspension

TECH DEPARTMENT

- a. All race cars entering the pit area are subject to a Safety Inspection. It shall be the responsibility of the Driver to see that his/her car is inspected before going onto the race track. Passing of safety inspection does not mean the car has been deemed legal within the division rules.
- b. All cars must start the race meet with all body panels in place. Body components found to be illegal must be replaced before that vehicle will be allowed to compete. Removal of an illegal component once the race meet has begun does not make that car legal.
- c. No equipment will be considered as having been approved by reason of having passed through inspection undetected. Any equipment which does not meet specifications or tolerances may not be eligible for approval.
- d. All cars being weighed for compliance with technical rules will be weighed with each wheel centered on a scale, the driver belted in the seat and sitting in an upright position with his/her helmet on or resting in his/her lap
- e. SPEEDWAY 95 does not “pre-tech” cars for competition. It is the competitor’s responsibility to know and follow the rules for their division. However, Technical Inspectors shall have the right to inspect any car at any time. Refusal to submit to technical inspection is your right; however, car and driver will not be permitted on the track until cleared by the Tech Dept.
- f. To facilitate timely inspection, only the driver and one crew member shall be allowed in the inspection area, at Tech discretion.
- g. SPEEDWAY 95 instruments for weights, measuring and gauging shall be used in determining official results.
- h. Top five (5) finishers of every feature must go directly from the track to the inspection area. Going to your pit area first and/or refusing to submit to post race inspection the vehicle will be considered illegal resulting in a disqualification – forfeiting position, purse, trophy, and points for the race event. Tech also has the option to pull one random position to go to tech also.
- i. At the time a competitor wins their third (3rd) feature win or any time thereafter, they may be subject to an in-depth engine inspection. Any competitor refusing engine inspection will be disqualified – forfeiting position, purse, trophy, and points for the race event. Neither the car nor competitor will be allowed to race again until they submit to required inspection.
- j. Any car/driver receiving a HEAT race tech inspection penalty will be disqualified from the heat finish order, and must start that race event’s feature at the tail end of the field (behind ALL other cars)

- k. Any car/driver receiving a FEATURE race tech inspection penalty will be scored in LAST PLACE in the final official finish order for that race event. The driver will be assigned 62 lineup points toward their handicap, and must start their next race event appearance at the tail end of the field (both heat AND feature)
- l. APPEALS – All drivers who receive a tech penalty are eligible to submit an appeal. The driver must request an appeal with track management, Competition Director, or Tech Director within 48 hrs of the infraction. Once an appeal hearing is scheduled, the driver's case will be heard by a panel that will include the Tech department, Track management, and aprox 3 neutral panel members (former or current drivers not in the same division). Once the panel has made their ruling, the matter will be considered closed. NOTE: If a driver, their team members, or family members go to social media to complain about SPEEDWAY 95, its tech department, or management about a tech penalty and/or on track penalty, they waive their rights to any appeal and all penalties will be deemed final.
- m. Technical rules that are changed during the race season will be posted on the racetrack website at www.speedway95racing.com under Driver Info, rules.

TEARDOWNS

- a. Management or the Technical Committee may request a technical inspection of any car at any time. Refusal to submit to technical inspection is considered out of stock and will cause loss of purse and all accumulated points. Top three finishers in each feature must go directly from the track to the inspection area. If you go to your pit area first, you will be considered illegal and disqualified. Management reserves the right to exclude anyone from the inspection area at any time. Only those persons approved by the Technical Inspector are allowed in the inspection area.
- b. When there is a specific question by other competitors in the same division regarding the legality of a particular part of a particular car, the following procedure must be followed in order to have the Tech Committee inspect that car.
- c. One driver/car owner competing in the same race meet as the car in question must notify the Pit Steward at least 15 minutes before that race that they desire an inspection of that particular part of that particular car. The driver or owner requesting the inspection must present \$500.00 and a technical inspection request form.
- d. If the car in question is found to be illegal, the money will be returned to the petitioner(s) and the car will be disqualified from the race meet. If the car is found to be legal, the money will go to the owner of the car in question minus the cost of the technical inspection. Only one (1) petitioner will be allowed to observe the inspection.
- e. Drivers of cars found to be illegal following an inspection by the Technical Committee will lose all points accumulated during that race meet. Any purse monies won during the race meet at which the car was found illegal will be forfeited. **Tech has the option to give last place points and loss of award money to a car/driver that has an infraction to the rules that would not warrant a disqualification.**
- f. Any car found to be in violation of technical rules may be subject to reinspection before it will be allowed to compete again. For the purpose of rule enforcement, the term "STOCK" will mean of original equipment manufacturer (GM, Ford, Chrysler, etc.) Any aftermarket parts are allowed at the discretion of the Technical Inspector.

FLAG RULES

- a. The Race Director, Flagman, and other track officials shall be responsible for the enforcement of the Flag Rules. If any driver disobeys any flag, their punishment may include a full event disqualification and forfeiture of purse money for the race event.
- b. *GREEN FLAG* – The green flag shall start all races and means that the track is clear. On the original race start, or any restart, no passing will be allowed before cars pass the start/finish line. Any car that jumps out of line or jumps the start by passing cars before reaching the start/finish line when the green is thrown shall be penalized. See “Penalties” section of general rules.
- c. *BLACK FLAG* – The black flag means that the driver must report to the pit area for further direction from track officials. This flag directive must be obeyed immediately. The black flag will be utilized for disqualifications or infraction of race rules. It will also be used for mechanical trouble that track officials feel could make the car dangerous to its driver and others. Failure to obey the black flag may be punishable by disqualification and/or further discipline as deemed appropriate by track management.
- d. *YELLOW FLAG* – The yellow flag means caution, and the track is not clear. When the caution flag is displayed, drivers are instructed to slow down, fall in a single line (nose to tail), with no passing. Any race that is in the process if the white flag lap will continue to the checkered flag, but the yellow flag may be displayed during the lap as a safety measure to warn competitors of a situation. Cars will be lined up for the restart according to their scored position on the last completed Green Flag lap, except for those cars designated to go to the rear of the lineup. A lap is considered completed when all cars competing on said lap have crossed the start / finish line. Any car going in the pit area while under caution will have the start the rear of the line up. We do not race to the yellow flag. In the event of a yellow flag on the final lap all cars must maintain their running position and safely cross the line. The running order will be frozen at the time of the yellow flag and the race will be considered complete.
- e. *RED FLAG* - The red flag means danger and stop at once. When the red flag is displayed all cars must stop immediately, regardless of their position on the track. Cars may not move until/unless directed by track personnel. Any competitor that fails to stop under a red flag situation may be parked for the remainder of the race event.
- f. *BLUE FLAG / YELLOW STRIPE* – The blue flag with yellow stripe is the passing flag. The flag may be shown to cars that are in danger of being lapped by the race leader. Said cars are asked to move to bottom or top of the track and hold their line until the lapping car(s) have passed.
- g. *WHITE FLAG* – The white flag means that the race is entering its final lap.
- h. *CHECKERED FLAG* – When the checkered flag is given to the lead car, the race is officially over. All following cars will continue to race to the checkered flag and then slow safely upon completion of that lap.
- i. *HALFWAY* – Will be shown with the yellow and green flags crossed in a “X”.

LINE-UP PROCEDURES

(Unless otherwise noted, these procedures apply to all SPEEDWAY 95 divisions.)

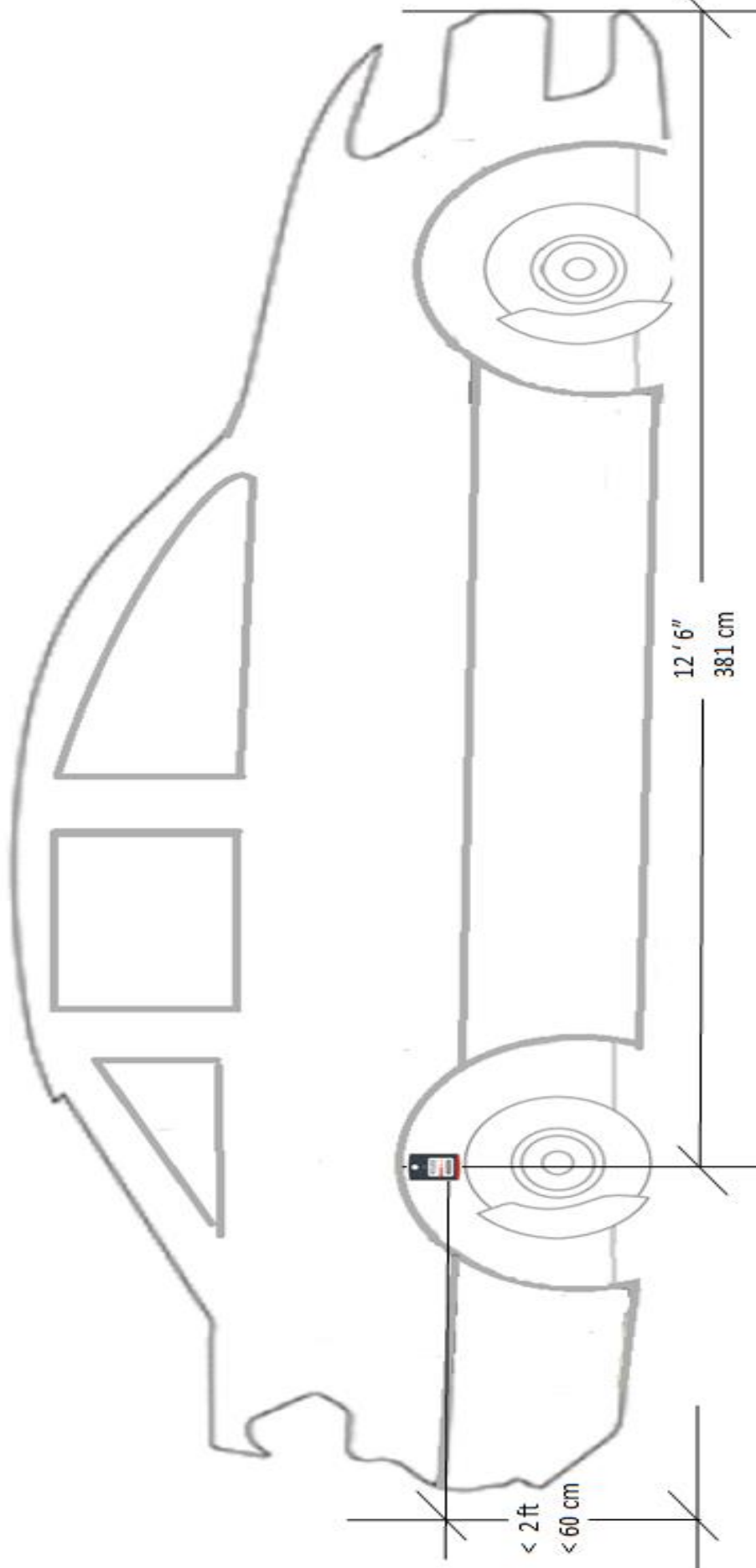
- a. On the first night, all drivers draw for heats. Qualifiers redraw for starting positions. One heat - 5 redraw; two or five heats - 10 redraw; three, four or six heats - 12 redraw.
- b. For all future track points nights scheduled for the season, heats are lined up by driver's three-event SPEEDWAY 95 point average, stagger inverted, lowest point average to front, highest point average to rear. Point averages are figured by driver's average SPEEDWAY 95 points earned in driver's three most recent appearances in weekly points events at the track. New drivers carrying no point average start at the rear. Two or more drivers having no average or same point average will be lined up at the discretion of officials.
- c. "B" mains are lined up straight up from heats with highest finishers to front.
- d. When one heat is run, the top five qualifiers will be inverted for feature lineup according to three-event SPEEDWAY 95, point average. When two or five heats are run, the top 10 qualifiers will be inverted for feature lineup according to three-event SPEEDWAY 95, point average. When three, four or six heats are run, the top 12 qualifiers will be inverted for feature according to three-event SPEEDWAY 95 point average. SPEEDWAY 95 and the race director reserve the right to relocate a driver from an inverted position to the rear of the field if deemed necessary for the safety and well-being of all involved. Remainder of "A" feature is lined up straight up from heats and/or "B" features.
- e. A driver that qualifies but has no point average shall be lined up in the last invert position. Should there be more than one driver with no point average, positions will be lined up at discretion of officials.
- f. Driver does not lose point average for missing any race nights. Once the driver establishes a point average, the driver maintains a point average for the remainder of the season.
- g. There are no double feature nights allowed, excluding rainout make-up features. A program is not considered a rainout unless four races have been completed. If the feature is rained out and will not be run at a later date, all drivers are to be awarded last place points.

TRANSPONDERS

- a. Are required for all divisions. Transponders may NOT be shared on different cars on the same race night.
- b. Transponders need to be registered when registering and drawing for position.
- c. Transponders MUST be on for practice. If you do not want your lap times or other stats shared during practice there is a way in the account settings to shut those off, but the transponder must be on the car to make sure it is working. If it is not working, you will not be scored in the race.
- d. Any car that refuses to leave the racing surface to have a transponder placed on the car will not be allowed out during the next practice. If it is the last practice, and you refuse to leave the race surface to have a transponder placed on your car, you will only receive half points for the heat race. Any car that does not have a Transponder for the Heat or Feature Race will not be scored.

Transponders

Placement will be 12' 6" from the front bumper



Late Model Rules

Bodies:

1. Must be stock appearing template type - ABC Specs
2. ABC Gen 6 Late Model FiveStar™ Body and AR Bodies and reskin kit are allowed.
3. May be constructed of steel, aluminum or fiberglass. No carbon fiber.
4. Full front and rear windshield constructed of a minimum eighth 1/8" inch Lexan. Windshield must be braced to prevent any deflection and/or distortion at speed.
5. "A" post mounted windows up to fourteen (14) inches long at the top of door and 90° straight.
6. Minimum roof height is forty-seven (47) inches as measured in the center and ten (10) inches rearward of the windshield. Roof may drop one (1) inch back toward the rear windshield.
7. Maximum spoiler size six- and one-half inches x sixty (6½ x 60) inches. Supports or bracing in front of the spoiler will not be allowed.
8. Spoiler must follow contour of the rear trunk lid or body shell with a maximum height of forty-one and one half (41½) inches measured from the ground to the top of the spoiler. No portion of the spoiler at its base may extend beyond the area where it connects with the body or trunk lid.
- 9. NO Air flow control devices.**
10. Hood and deck lid must be tethered to the roll cage.
11. Lifting/towing chains or cables must be installed both at the front and the rear of the race car are required. Absolutely No Exception.
12. All body measurement will be checked with Five Star Referee.
13. No panning of any kind to underside of car or airflow devices

Frames:

1. Must be square steel tubing a minimum of two (2) inches x three (3) inches.
2. Rear clip to be square tubing only.
3. One-hundred and two (102) inch wheelbase minimum.
4. No panning of any kind to under side of car or airflow devices.

Weight:

1. All cars being weighed for compliance with technical rules will be weighed with each wheel centered on a scale, the driver belted in the seat and sitting in the upright position with his/her helmet on or resting in his/her lap, with car being race ready, full of fuel.
2. Built engine: 2,750 lbs.

3. GM Crate engine #604 without cam change: 2,700 lbs must run a Speedway 95 approved adapter/restrictor plate. Part Number 1480 supplied by RPM Engines, Georgia VT (802) 524-7406, will be available for purchase at the racetrack.
4. GM Crate engine #602, #603 and #609: crate motors allowed 2,700 lbs.
5. 602,603,609 2,700 lbs
604 with restrictor plate #1480. 2700 lbs
604 without restrictor plate and built motors. 2750 lbs
602 with alliance seals may run a Holly HP 600 #80540 or holly HP 650 #80541 must pass tech tools
6. Left side maximum is fifty seven percent (57%) before and after race
7. All ballast weight must be located outside of the driver compartment with each piece attached with a minimum of two (2) three eighths (3/8) inch bolts, washers and locking nuts.
8. All weight must be lead with each piece painted white and display car number.
9. Any car with 3 wins will have 25 lbs added to the right side of the car, between the axels and will go up 25 lbs after every victory there after.

Weight Penalties/Allowances:

1. Any engine with stock clutch 30 lbs assembly.-50 lbs.
2. Engine set back up to two (2) inches +50 lbs.
3. Aluminum body shocks meeting all other requirements +20 lbs.
4. Min weight 2,650lbs.

It is the driver's sole responsibility to know what he/she has for a motor and may be checked at any given time. If motor is checked and illegal it will be at owner's expense if legal is at the track's expense.

Suspension – Steering:

- 1.Any steering system allowed.
- 2.Any sway bar system allowed.
- 3.Tubular upper and lower control arms allowed.
- 4.Any steel body shock allowed with a common retail value not to exceed \$300.00 each. For clarification: One shock and one spring per corner. MUST remain completely unaltered with product number and manufacturer visibly identifiable
- 5.All shocks subject to claim by Speedway Management three hundred dollars (\$300.00) each at any time.
- 6.No gas or adjustable shocks except for Koni 30-7436, Koni 30-7499, Koni 30-7325, Koni 30-9325, Koni 30-9436, Koni 30-7647, Koni 30-1309.
- 7.Bump stops allowed – disc type only. NO COIL BINDING.
- 8.No traction control of any kind.
- 9.No driver-controlled chassis adjustments except for brake bias.
10. Track width not to exceed sixty-six and one half (66½) inches as measured at spindle pin height.
11. There is no ride height rule, but body must meet all ABC specs. **No ride height on chassis but min body height from body to ground is 3".**

Wheels – Brakes – Tires:

1. Steel racing wheels up to eight (8) inches wide as measured from inside of bead to bead.
2. Any brake systems allowed. All four wheels must have brakes.
3. Tire pressure bleeders shall be allowed.
4. Speedway 95 Official Track Tire: American Racer 955.26.5 and American Racer 955.27.0 on EITHER SIDE with any combination. Tires must be purchased from Speedway 95 track representative. Once tires leave the tire truck, ABSOLUTELY NO RETURNS.
5. Each car must have four registered tires on the car for the heat and the feature. Speedway 95 tech officials shall maintain an inventory of all registered tires for each Late Model team, with all tires registered to the car number. Late Model teams shall be responsible for their own tire management.
6. Late Model teams arriving to race after the 2nd race meet shall have the option to purchase a maximum of four (4) new tires to begin their registered tire inventory and on actual Late Model racing events thereafter, shall have the option of purchasing a maximum of one (1) new tire for their registered tire inventory. Should the option of one (1) new tire purchase not be exercised, the option shall be forfeited. **All tires shall be registered to the car number. Purchased tire shall be registered to that team's car number and be mounted on the car and run on the night it was bought on in both the heat and feature.** (on a 50 lap race there will be 2 tire allowed per event and on 100 lap race there will be 4 tires allowed per race)
7. Should a tire be damaged during a race event, it shall be the responsibility of the race team to present the damaged tire that race meet, and in a timely manner, to Tech for inspection. All tire replacements must have a written record with Tech regarding inspection and approval for replacement. Only tires with 50% or more tread will be eligible for replacement. It is speedways sole discretion on replacement of any damaged tire.
8. Speedway management reserves the right to modify tire rules in conjunction with the division's needs and concerns.
9. Absolutely no tire softener or any chemicals used while mounting tires to rims may be used. Durometer testing shall be done at random. Any tampering with tires will result in disqualification.

Absolutely no tire softener.

If tech suspects tire softener all tires will be dismounted and confiscated, and you will be disqualified. If protested a sample of the tire will be sent out for evaluation. The driver will be responsible for all costs of the testing if it is found positive.

All Engine Guidelines:

1. Gage legal Holley #4412 only.
2. Any non-tapered spacer/adaptor up to one (1) inch thick.
3. Any exhaust system that utilizes an as manufactured muffler(s).
4. No crank trigger ignition system.
5. No electric or belt driven fuel pumps.

Non-Sealed Engine Guidelines:

1. GM, Ford, Chrysler small blocks only. Must be cast iron.
2. No portion of piston can protrude above block deck surface.
3. Maximum overbore = .060"
4. Maximum compression ratio = 11:1
5. Stock OE type crankshaft only. OE stroke. (3.480 inch for GM) No modifications except as needed for balancing. Exception – Ford 302 based engines may utilize a 3.400 inch stroke crankshaft to attain a displacement of 347 cubic inch.
6. Connecting rods must be magnetic
7. Any flat tappet camshaft. No mushroom lifters. Chain driven only.
8. Stock lifter bore size only.
9. Built engines are required to have cast iron heads only. Maximum valve size = 2.02 intake/ 1.60 exhaust. No port or combustion chamber modifications.
10. Roller rocker arms allowed. 1.5/ 1.6 ratio only. No shaft rockers.
11. Wet sump oiling only. External/remote filters and coolers allowed.
12. GM small port Bowtie Vortec #25534351 allowed in as produced fashion. Ford and Chrysler performance division heads to be determined on a case by case basis.
13. Edelbrock performer intake only. #2101, #2104, #2116 and equivalent for Ford and Chrysler.

Crate Engine Guidelines:

Must follow GM Specs

1. GM # 602 allowed
2. GM ZZ-4 based engines – ie # 603 and #609 allowed.
3. Ford # M60075347JR allowed must add 10 lbs per side on front clip as close to upper control arms as possible.
4. All crate engines must have a full seal set provided by the OE manufacturer or Red Line of Hollowell, S&S Performance of Farmingdale and Thayer's Automotive of Hermon.
5. GM # 604 engines without a cam change will be allowed.

Cooling – Battery - Exhaust:

1. Any radiator that fits under the stock hood is allowed.
2. Water and a cooling additive only. No antifreeze allowed.
3. Must have a minimum one (1) quart capacity sealed overflow container.
4. Car must not have fluid leaks of any kind.
5. Battery must be in a sealed battery box and properly secured.

Drive Train:

1. Any rearend, may be locked. Rearend safety hubs recommended.
2. Any OEM three (3) or four (4) speed transmission is allowed.
3. OEM type T-10 is allowed.
4. Must have two (2) forward gears and reverse.
5. No aftermarket transmissions except as noted above.
6. No internal to the transmission clutches.
7. Cluster gear must stay engaged to the main shaft at all times.
8. No straight cut gears allowed.
9. Minimum clutch diameter = 7.250 inches
10. Magnetic flywheel only.
11. Driveshaft must be painted white.
12. Driveshaft loop required near the front of the driveshaft with material specification minimum of one-quarter (1/4) inch x two (2) inches. Rear driveshaft loop is recommended.

THE TECH COMMITTEE RESERVES THE RIGHT TO ADD OR SUBTRACT WEIGHT AT THEIR DISCRETION FOR EACH ITEM NOT MEETING SPEEDWAY 95 SPECIFICATIONS.

Tech has the option to give last place points and loss of award money to a car/driver that has an infraction to the rules that would not warrant a disqualification.

THE TECH COMMITTEE MAY ADD OR SUBTRACT WEIGHT IN THE INTEREST OF COMPETITION.

Any alteration to any part of the car not specifically covered in these rules must be cleared with the technical inspector or it will be considered illegal. In other words, IF IT DOESN'T SAY YOU CAN DO IT, YOU CAN'T.

Any specification not covered by these rules may be decided upon by tech management and made a rule for the rest of the season

Limited Sportsman

Chassis

1. **Chassis:** Any American branded, four (4) passengers, V-8 front engine, rear wheel drive, full frame, or Uni-body car originally mass produced. Engine and body must be of the same manufacturer as the chassis used.

a. **Weight:** 2,900 lbs. race ready with driver, with a maximum left side percentage of 56/44%.

b. **Ballast Weight:** Must be in between tires and painted white with car.

c. **Bumpers:** Stock front and rear bumpers may be used. Approved tube type front and rear bumpers may be used.

d. **Design:** Minimum four (4) point roll cage is required. In addition to the required four (4) point roll cage, the structure may extend to the front as far as the radiator support and to the rear to sufficiently protect the fuel cell. The left side must have a minimum of four (4) horizontal bars with spacers. The right side must have a minimum of three (3) horizontal bars with spacers or an "X" and two (2) horizontal bars.

e. **Door Plate:** A minimum of sixteen (16) gauge steel plate of sufficient length and width as to protect the driver must be welded to the roll cage.

f. **Fire Walls:** The driver's compartment tub (to the right and to the rear) to be constructed from a minimum of 22-gauge steel. The driver's foot box and firewall to be constructed of a minimum of 22-gauge steel.

g. **Frame:** Stock frame rails may be replaced with 2" wide by 3" high tubing from the rearward-most point of the stock front clip. Stock dimensions for the year, make, and model of the car must be retained. Reinforcing with tubing and 1/4" plate is allowed to the stock frame.

h. **Front Clip:** No tube type front clips allowed. Factory production full 1968 or newer perimeter American passenger car front frame clip only. No modifications to clip permitted except for upper control arm mounting, radiator clearance and mounting and trimming of forward front cross member for center link clearance.

i. **Horns:** May be fabricated using 2X3" tube. The steering box and idler arm MUST be in Factory location.

Crossmember:

NO cutting, raising, shortening, of factory front cross member.

i. **Upper Control Arm Mounts:** Mounting point may be relocated.

ii. **Spring Pocket:** May be opened for access.

iii. **Cross Member:** No alterations of OEM engine cross member.

iv. **Suspension Points:** No alterations to OEM mounting points.

Fuel Cell:

must be a steel container with a check/rollover valve in the vent line; secured with two (2) straps front to rear and two (2) straps left to right. The minimum dimension of strap material is eighth (1/8) inch x one and one half (1½)

inch. Fuel cell may not be offset. Cell must be a minimum of 8" above the track and mounted in a safe manner from rearend tube to front of cell

Height:

Minimum frame height from ground is 4".

- a. **Interior:** Interior area of car must be completely enclosed in respect to the engine compartment, track surface, tires, and rear (fuel cell) compartment. This may be constructed of aluminum with a minimum of .040 thicknesses.
- b. **Pipe:** Minimum roll cage tubing specification one and three quarter (1¾) inch x point zero nine five (.095) inch DOM or seamless tubing.
- c. **Rear Clip:** Rear frame may be altered to accept leaf or coil springs. Underslung chassis must add 2X3 up and over rear axle or mount a 10lbs weight at or near the top rear shock mount on both sides. d. **Seat:** Aluminum racing seat with right and left head supports. Seat must be mounted and attached securely to the cage. A support brace must be installed at the rear of the seat. The drivers head should be below halo and clear of all roll bars.
- e. **Seat Belts:** Mounted with three eighths (3/8) inch bolts minimum Grade 8 washers and locking nuts. These must be attached at two (2) separate points on the frame, equal distance from the seat back and not less than twelve (12) inches apart. (See manufacturer instructions for details).
- f. **Steering Column:** A fabricated steering column must have a minimum of two (2) Ujoints positioned so as not to allow the steering wheel to be pushed rearward in the event of a collision. Must have steering column stop. All steering wheels must have a center pad.
- g. **Welds:** All welds are to be of high quality and must completely surround the joint. Absolutely no butt welds or sharp edges anywhere.
- h. **Battery:** 12-volt Battery mounted and covered in a secure fashion mandatory

Wheelbase:

The minimum wheelbase allowed is 101", without tolerance. Absolutely no modifications except as specified in this rule book.

Motors and Running gear

Crate Option 1)

GM crate engine p/n 88958602 is allowed in full untouched factory form with 4412 Holly 500

- Carburetor: Stock! Unaltered carburetor. For all crate
- Oil Pan: A 7" oil pan is allowed.
- Height: 12" crank height while at a 4" frame height must be maintained

Built Motor Option 2)

American made engines only. No V-6 engines. No aluminum or other soft metal blocks or heads allowed. Crankshafts must be stock type, dimension and weight. No stroking or de-stroking permitted. Normal

engine balancing will be the only modification to this piece allowed. Blocks must be stock production with standard external measurements in all aspects. Blocks can be dressed. The maximum allowable compression ratio will be 10-5-1. The maximum cubic inches listed below will be allowed for the following engine makes:

1. Chrysler (Dodge & Plymouth) 360 ci + .040

2. Ford 351 ci + .040

3. General Motors 350 ci + .040

Balancing:

Normal engine balancing for pistons and connecting rods will be defined as follows: The piston or rod in a set of eight having the least stock weight will be used, and the remaining pistons or rods will be matched to it.

Heads:

All cylinder heads must be cast iron, original equipment manufacturer production. The only exceptions to this are noted below with a (++) symbol.

Specifications as follows:

- General Motors maximum intake diameter 1.94"; maximum exhaust diameter 1.50" Chrysler 340 ci motors maximum intake 2.02"; maximum exhaust diameter 1.60" Chrysler 360 ci motors maximum intake diameter 1.88"; maximum exhaust diameter 1.60" Ford Winsor motors maximum intake diameter 1.84"; maximum exhaust 1.54"
- ONLY THE HEADS LISTED BELOW ARE ALLOWED:
- General Motors casting #'s 041, 186, 291, 370, 441, 461, 462, & 492. Vortec Heads 906/062 Allowed (The 492 casting must be G.M. part)
- World Products Inc. S/R 043600-1 and 043610-1.
- Ford Winsor motors may use Ford Motorsports casting #GT40-M6049-L302
- All General Motors heads must use 1.250" diameter valve springs.
- Any cylinder head not listed must be approved by the management of Speedway 95 before being used.
- All cylinder heads must have stock dimension valves and valve springs for the casting used. No titanium or neckdown or pro-flow type valves allowed. Valve stem length, diameter and keeper groove locations must be stock. Stock type retainers must be used. No angle milling, port matching, or polishing allowed. No gear drive assembly allowed. 3/8" screw-in studs, pinned studs, polylock nuts, and push-rod guide plates are allowed.

Lifters:

All engines must use hydraulic lifters no mushroom types or roller lifters allowed

Camshaft:

Hydraulic lifter camshaft with a maximum lift of .450. The lift will be measured at the valve stem. No gear drive assemblies allowed.

Rocker Arms:

Roller rockers are permitted but must maintain a max. .450 cam lift. A maximum 1:5 rocker arm ratio is allowed.

Pistons:

Any “Flat Top” piston with a minimum of two valve reliefs will be allowed. Floating of the wrist pin is optional. Normal engine balancing only. No part of piston shall extend past the top of the block or deck.

RODS:

No aluminum or custom steel rods allowed. No grinding, lightening, or polishing of any kind is allowed. The installation of aftermarket rod bolts and nuts is allowed. 6” rods are not allowed.

Carburetors:

Carburetors are limited to one stock 4412 Holly 500 C.F.M. two barrel. No alterations are allowed except the removal of the choke plate. Epoxies or other devices to alter the flow in the manifold are not allowed. Spacers or high rise manifolds are not allowed. Adapters not exceeding 1 1/8” in height are allowed. Adaptors may not be grooved, tapered, or reworked in any manner. Gaskets shall not exceed 1/4” thickness. Crate 602 Option (part 8895-8602 is permitted with Holly 4412 500 CFM carburetor and must be sealed)

Intake Manifolds:

Intake manifolds must be stock Edelbrock 2101, 2116, 2716 Performer. Ford Appropriate style to match GM. They may not be modified in any manner.

Ignition:

A stock ignition system is allowed. No external amplifier-type systems are allowed. Ignition- and rev-limited devices are not allowed.

Motor Mounts and Setback:

The original motor mounts may be replaced by steel mounts. They must bolt to the original location, both frame & motor. THERE WILL BE NO MOTOR SET-BACK ALLOWED. Engine plates are not allowed.

Exhaust:

Stock manifold with cherry bomb mufflers (are) allowed. Steel headers are allowed. Headers must be Limited Sportsman approved with 1 5/8” tubes and a maximum 3” collector. Cross-over headers are not allowed. Mufflers required.

Bellhousing & Clutch:

Scatter shields are recommended, but not mandatory. Stock Bellhousing with a hydraulic release bearing allowed. Flywheels and clutches must be stock production OEM. This includes weight, size, and physical appearance. Aftermarket high-performance types are not allowed. All cars must have a 1” hole in the bottom of the bell housing to allow for clutch inspection. No reverse mount bell housing and starter kits allowed.

Driveline

a. Rarends: Rear-ends must be either locked or open single-track. Stock housing for make and model any gear ratio may be used. The rear axles may be replaced on both sides of the car with an aftermarket axle using a larger bearing. The replacement must fit in the original rear-end housing. No ratcheting or torque-sensing devices or clutch packs allowed.

b. Ford 9" Option: A Stock steel Nine-inch rear may be used

1. Gear ratio 588 Max
2. No machining or lightening of ring gear or pinion.
3. Mini spool or open differential
4. No Camber or toe built in
5. Steel drive plate and axel caps
6. Minimum Axel diameter 1.200 and weigh 7lbs

c. Driveshaft: Steel driveshaft (singular) only. Driveshaft must have a safety loop of 1/8 thick buy 2" wide.

d. Transmissions: Stock 3 or 4 speed transmissions allowed Transmissions must contain all the original gears for the unit used. No straight cut gears.

Body

No Five Star Gen 6 or Revolution body components. Any Combination of ABC, fiberglass, steel, or aluminum duplicates body parts can be used but must match all ABC dimensions. Any body style allowed but must meet all dimensions for make and model and ABC heights and dimensions.

a. Roof Height: Roof min height is 47' measured 10' from the center of the windshield.

b Height: No part of the body (including side skirts and nose piece) may be closer than Four (4) inches to the ground.

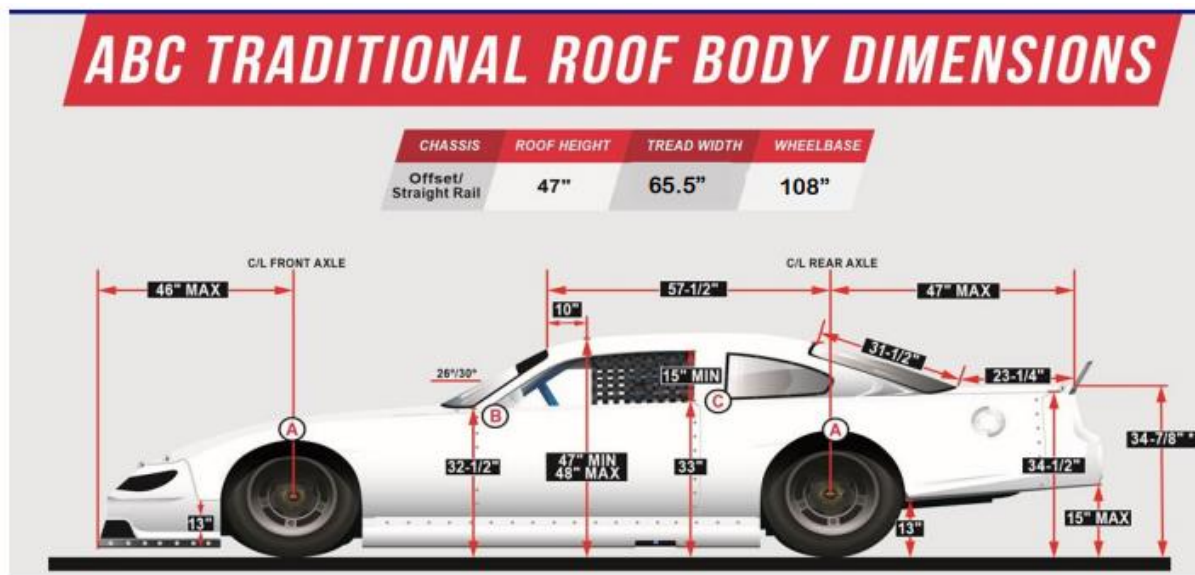
c Spoiler: Spoiler must be mounted on the bumper cover in the center left to right with a (maximum) dimension of 6 1/2" tall and 60" long (wide) made of clear Lexan no side bracing or boxing of any kind. Maximum height is 41" from the ground.

d Air flow devices: No air flow devices added to the A or B pillar, or vent windows allowed Rear quarter windows are optional and must be clear lexan.

e Hood: Maximum hood cowl opening is 2" No scoop or openings in the hood for air cleaner

f Bumper Covers: No modifications allowed to front or rear bumper covers.

g Deck Lid: The rear deck lid must be hinged, pinned for viewing and safety access. No solid riveting.



a. Rub Rails: Lexan rub rails are permitted but must remain between wheels.

b. Under panning: No panning is allowed at the front or the rear of the car. Radiator duct work can be no wider than radiator.

c. Windows: A full windshield is required. The windshield must be a minimum of 1/8" thick polycarbonate material such as Lexan. A minimum of two windshield braces are required. Rear windows and quarter windows are optional and must be made of the same material as the windshield. Window installations are subject to the Tech Inspector's approval.

Suspension

a. Adjusters: No driver-controlled chassis adjustments.

b. Control arms: Stock, unaltered lower "A" frames. Cannot be moved. GM intermediate metric chassis may use any stamped steel OEM type lower control arm Upper: Tube type upper "A" frame allowed and can be moved and fabricated. Control arm bushings are limited to stock, steel, or neoprene type only. No spherical bearing or Heim end bushing type permitted.

c. Ball Joints: Screw in ball joints allowed

d. Drag Link: Stock or Howe Type

e. Materials: No aluminum, titanium, or carbon fiber parts are permitted. Exception: Tie rod adjuster sleeves.

f. Panhard: Coil spring rear suspension may be converted to a three-point system with a Panhard bar, No quick Adjusters.

g. Steering: Steering box must be OEM or utilize 700 series steering box only. No aftermarket design steering box. Must be mounted with original bolt pattern.

h. Pitman & Idler Arm: OEM or Howe Type

Shocks:

Any non rebuildable or non adjustable. Retail less than \$150. No coil over shocks, No bump stops of any kind.

b. Spindles: OEM cast spindles

c. Hubs: Stock OEM type hubs or a Steel aftermarket safety hub may be used on the right front.

d. Springs: Conventional coil spring with a minimum five (5) inch inside diameter, no coil over suspension allowed. All springs must be magnetic steel. Minimum front spring rate five hundred (500) pounds / inch.

e. Front: No fiberglass graphite or exotic springs permitted.

f. Rear: Leaf spring, no fiber glass lift bars. No torsion bars in rear. Adjustable leaf spring shackles, sliders and lowering blocks allowed.

g. Weight Jacks: Must be centered with coil springs. No hydraulic, ratchet, or electric weight jacks anywhere in or on car. "No Bird Cages".

h. Sway Bars: OEM or Howe type one piece front sway bar allowed maximum (1 3/8"). Must mount in original location.

i. Third Link: The rear center link may be a solid link No spring loader third or rubber links permitted.

j. Trailing Arms: No spring loaded or rubber biscuits. Must be equal length. No center pull trailing arms

k. Tread width: The maximum tread width allowed will be 65 1/2". The overall width from outside to outside of wheel rim bead flanges shall not exceed 73 1/2" inches, as measured with the SPEEDWAY 95 tread width gauge.

Over 65 1/2" will add 50 lbs up to 66 1/2". Anything over 66 1/2" will not be allowed.

Tires and Wheels

a. Tires: 7" American Racer from Speedway 95.

b. Bleeders: Absolutely no tire bleeding devices allowed

c. Wheels: Steel 8" or smaller racing wheels will be allowed. Minimum 5/8" wheel studs are required on the right side of the car.

d. Brakes: All cars must have four working brakes on the car. Disc brakes are allowed. Drilling of rotors is not allowed. Clutch cylinders are allowed. OEM single piston calipers and rear rotors to maintain a min thickness of .810 and be straight veined. No scalped rotors allowed.

e. Transponders: Transponders are mandatory and must be mounted 12' 6" from the front bumper. (see SPEEDWAY 95 website for description)

Speedway officials may assess weight penalties for race equipment deemed not in compliance with the rules. Race equipment will not be considered as approved by reason of having passed through inspection at any time – or any number of times – unobserved or undetected. Any race equipment which does not conform to specifications contained in the rule's listings, or that is not otherwise approved by Speedway 95, may not be used in competition.

Any car with 3 wins will have 25 lbs added to the right side of the car, between the axels and will go up 25 lbs after every victory there after.

Street Rules

Bodies:

Stock Appearing Duraflex™, FiveStar™, AR Bodies™ and aluminum bodies are allowed and **NO FIBERGLASS BODIES**, must be street stock bodies, **NO WEDGE BODIES**. Cars must be American made passenger production six (6) or eight (8) cylinder cars 1970 to present. All chassis must maintain stock wheelbase for frame utilized. Cars must be street legal for make and model. No convertibles, Vans, Station Wagons, or pick-up truck type vehicles, Front or rear wheel drive allowed. No four-wheel drive vehicles. Bodies may be interchanged with chassis, but must be of same make, example, Chevy body on Chevy frame Etc. Stock after-market panels allowed per stock specifications. Rest of body must match model of roof, including front fenders, must be stock appearing. only exception is front and rear stock type vehicle plastic bumper covers of any model allowed. No dirt or wedge type bumper covers. Floorboards must be stock on driver's side. Passenger side may be fabricated but must remain fully closed in and of similar height to floorboard. No tunnel or raised tin fabrication. Firewalls may be fabricated from .22 gauge steel but must remain in stock location. Fabricated firewalls receive a 25 lb. weight penalty. Rocker panels may be removed but must be replaced by metal fabrication that keeps interior of car enclosed. Doors may be replaced with .22-gauge steel. Hoods may be replaced with fiberglass or aluminum. Air Breathers cannot protrude outside of hood. Hood cannot be cut open. Additional hood scoops allowed but only opened in the rear, No Air Induction Scoops ALLOWED. Rear deck lids must be in place and of stock length and installation. Minimum wheelbase of any car 101". Inner body panels may be removed. Spoiler 5" x 60". No side boxing. Spoiler cannot extend past top of bumper cover. Max spoiler height 43" from ground to top of spoiler. Modifications to the rear body is allowed to achieve this height only. No lengthening of tail pieces or fender shelving added, no more than 3" shelving on each side or any other body modifications allowed. Maximum length for Monte Carlo 40" Maximum quarter panel height 38" Roof Height minimum 48"

No panning of any kind to under side of car or airflow devices except for what is allowed in the rule book

All cars must start the race meet with all exterior body panels in place.

Vent windows allowed maximum 12" length

Frame:

Must have a four-point roll cage but a full roll cage is suggested. Square tubing allowed from center of rear-end back, must be in stock position. Roll cage cannot be set back any closer than 24 inches from center of rear end. Wheelbase must match suspension for year make and model of chassis. All cars must have tow hooks on front and rear. Center section of chassis may be tied together above the floorboards according to tech specs.

Weight:

Must weigh a minimum of 3,000 lbs. with driver in the car. Maximum Left side weight is 54% (54/46). All cars being weighed for compliance with technical rules will be weighed with each wheel centered on a scale, the driver belted in the seat and sitting in the upright position with his/her helmet on or resting in his/her lap.

Fabricated firewalls receive a 25 lb. weight penalty. If 8" wheels must be 50 lbs. heavier (3,050 lbs. with driver in the car) with 53% left side weight. **Any car with 3 wins will have 25 lbs added to the right side of the car, between the axels and will go up 25 lbs after every victory there after.**

Suspension:

Front and rear jacking bolts allowed. 4th generation Camaro running coil over struts on car must have spanner nuts and must be doubled up so they are locked. Upper control arms on rear end may be re-drilled. Lower rear control arm mounts may be moved up or down, must remain stock. Stock replacement tubular upper A-frames allowed with a non-adjustable ball joint only, mounted in stock position on stock mounts 1" variance from stock length allowed. Bolt in Ball joints ONLY. Lower A-frames must remain stock make and model. Stock sway bars only in stock location, stock frame mounts may be adjustable on one side. Max diameter 1 3/8". Struts must be mounted in stock location. No high performance, adjustable or "take apart" shocks allowed, no split valve shocks. No reinforcement of brackets or suspension arms. Minimum height of any part of the body or frame must be 5" measured from the lowest part of the frame. All other suspension parts must be stock for make. Rear leaf spring cars may have adjustable shackles no longer than 6", lowering blocks allowed non-adjustable. Panhard bar and trailing arms allowed on full frame metric cars. Three (3) link rear suspension with stock ends ALLOWED. Adjustable MUST be welded solid. No bump stops or suspension limiting devices allowed. Leaf Springs MUST be steel. Strut shaft size 32mm MAX. Shocks must be steel and non-rebuildable retail value of \$125.00

Wheels:

May use aftermarket steel wheels. Track width no more than 64 1/2". Max. Maximum wheel width seven (7) inches. If 8" wheels must be 50 lbs. heavier (3,050 lbs. with driver in the car) with 53% left side weight. No Wide 5 Wheels.

Tires:

American Racer 7" 225 SH or H track tire, EITHER SIDE with any combination Tires must be purchased from the authorized tire representative. Once Tires leave the truck ABSOLUTELY NO RETURNS. **No chemicals may be used while mounting tires to rims.**

Absolutely no tire softener.

If tech suspects tire softener all tires will be dismounted and confiscated, and you will be disqualified. If protested, a sample of the tire will be sent out for evaluation. The driver will be responsible for all costs of the testing if it is found positive.

If any car does not meet these above rules weight will be added or penalty will be accessed.

Engines:

602 crate engines allowed with stock specs. Minimum, 13 ½" crank height measured from ground to center of bolt. No rebuilds.

GM max engine size 350 plus .060 over . Ford max 351w plus .060 over Chrysler 360 plus .060 over. Engine block cylinder heads must be cast iron and O.E.M. stock. Crank shaft must be cast or forged. No modifications or lightening. Minimum two valve relief pistons allowed. Stock steel connecting rods aftermarket stock replacement. Racing oil pan allowed. Cam shaft must be hydraulic flat tappet only. Stock lifter bore. Max. lift 450 at the valve. Stock type rockers 1.5 Ratio. Cylinder heads must be O.E.M. castings straight plug. World product head #4361 will be allowed. Vortex heads allowed. Max valve size 1.94 intake 1.6 exhaust. Intake must be stock O.E.M. cast iron 2 barrel or 2101 or 2116 aluminum ONLY may be used. ONLY 602 crate motors can run crate intake. Max compression 10.2:1.

Carburetors:

Stock Holly™ 4412 on built or 602 crate. No VDL's. No changeable air bleeds. Only modifications are the removal of choke plate assembly, jet change, power valve change and accelerator pump change. Rochester™ (2) two barrel allowed. Must use Mr. Gasket™ # 1929, #1937 or #1933 adapter ONLY. No 4 barrel carburetors allowed on any engine.

Fuel Cells:

May be mounted through trunk floor and securely fastened to the car. Fuel Cell minimum ride height 12" from the ground. No adjustable or sliding spacers.

Exhaust:

Racing mufflers mandatory. If the track deems your race car to be too loud, you will be required to run track muffler.

1 5/8" maximum underslung headers allowed stock manifolds no ram horn manifolds.

Glass:

All window glass (except for windshield) must be removed. All broken glass must be removed from the car before it will be allowed to compete. Quarter panel windows of clear 1/8" Lexan construction are permitted. No deflectors, or boxing of door opening area is permitted only Vent windows allowed maximum 12" length

Transmission:

Stock cast iron 3 speed. All gears must be working. Stock steel fly wheel Minimum 16 lbs. and stock clutch 30 lbs. total weight assembly. Automatic transmissions allowed. Stock torque converters only.

Rear End:

Locked rear ends allowed. Mini spool or welded. NO RACING BRACKETS ALLOWED. No cambered rear ends. 5:88 final drive ratio. Stock for make. 9" Ford allowed, no floaters.

Radiator:

No anti-freeze allowed. Water and Keep Kool™ only. All cars must have an overflow jug in working order.

Battery:

Battery may be moved to behind the driver's seat or remain in stock location

Brakes:

All cars must have four-wheel brakes in working condition. Four wheel disc brakes allowed no clamp on brackets. Stock Calipers. After market pedals and master cylinders allowed. No brake bias mechanisms allowed.

THE TECH COMMITTEE RESERVES THE RIGHT TO ADD OR SUBTRACT WEIGHT AT THEIR DISCRETION FOR EACH ITEM NOT MEETING SPEEDWAY 95 SPECIFICATIONS.

Tech has the option to give last place points and loss of award money to a car/driver that has an infraction to the rules that would not warrant a disqualification.

THE TECH COMMITTEE MAY ADD OR SUBTRACT WEIGHT IN THE FAIRNESS OF COMPETITION

Any alteration to any part of the car not specifically covered in these rules must be cleared with the technical inspector or it will be considered illegal. In other words, IF IT DOESN'T SAY YOU CAN DO IT, YOU CAN'T.

Any specification not covered by these rules may be decided upon by tech management and made a rule for the rest of the season.

Motorsports at all levels pose a risk to your wellbeing. Following the rules set for by Speedway 95 in no way guarantees or suggests your safety.

Sport Four Rules

Tube Chassis: Tube chassis must be rear wheel drive only.

Floor Pan Car: Must utilize the majority of OE/as produced vehicle structure. Outside of the main roll cage structure a basic front and rear hoop for crash & component protection is allowed.

Bodies:

Must resemble a stock body automobile in regard to size, shape, and contour.

Aluminum and Fiberglass bodies allowed and must add twenty five (25) pounds. Hood and deck lids must have all bracing removed and be tethered to the car.

Sun roof must be covered with sheet steel of similar thickness to existing roof structure and welded into place. No "T" tops. Original equipment windshield (safety glass) in good condition may be used. Minimum eighth (1/8) inch Lexan windshield is allowed with at least one (1) support strap installed close to center. All other glass and combustible material must be completely removed.

One (1) inside mirror no larger than three (3) inches x twelve (12) inches and one (1) left side mounted mirror no larger than five (5) inches x five (5) inches is allowed.

Spoilers may be a maximum of five (5) inches tall and fifty-eight (58) inches wide.

Bumpers are required to have a retention chain/cable at both ends. Rub rails mounted tight against the body are allowed.

Lifting/towing chains or cables must be installed both at the front and the rear of the race car are required. Absolutely No Exception.

No down force bodies or wedge noses. Back end of body must be enclosed. No open tails.

Eligible models:

1. Any mass produced [4] cylinder passenger cars.
2. Two or four door models permitted. No convertibles, minivans or trucks.
3. Front engines only.
4. No all wheel drives, all wheel conversions or 4x4 will be allowed.

Frame:

Any tube or stock type chassis shall be allowed. Tube chassis rear wheel drive only. No front wheel drive tube chassis allowed.

Pipes must be a minimum of one and one half (1½) inch x point zero nine five (.095) inch wall.

Frame tubing must a minimum of two (2) inch x two (2) inch x point one two five (.125) inch wall. Fabricated tube chassis wheelbase minimum ninety six

(96) inches.

Weight:

Car must be marked with CC's on the hood.

One (1) pound/cc with a twenty-two hundred (2200) pound minimum for fuel injected cars.

One (1) pound/cc with a twenty-one hundred (2100) pound minimum if under twenty-three hundred (2300) cc and carbureted.

Left side maximum is fifty-six percent (56%).

All ballast weight must be located outside of the driver compartment with each piece attached with a minimum of two (2) three eighths (3/8) inch bolts, washers and locking nuts.

All weight must be lead with each piece painted white and display car number.

All cars being weighed for compliance with technical rules will be weighed with each wheel centered on a scale, the driver belted in the seat and sitting in the upright position with his/her helmet on or resting in his/her lap. **Any car with 3 wins will have 25 lbs added to the right side of the car, between the axels and will go up 25 lbs after every victory there after.**

Weight Penalties/Allowances

Non-steel body.....+25 lbs.

Full tube chassis/no O.E. floor pan.....+50 lbs.

Double overhead cams.....+50 lbs.

Variable cam timing.....+50 lbs.

7" wheels.....- 50lbs.

Longitudinal engine w/iron head.....-100 lbs.

Longitudinal engine w/aluminum head.....-50 lbs.

Tube chassis cars with longitudinal engine placement will receive no weight break.

Wheels – Brakes – Tires

American Racer 13" or 14" Track tire or any radial tire no less than 60 series. No mixing of tires. Car must have all four racing tires or all four DOT tires. All tires on car must be same size, either all 13" or all 14". Track tires must be purchased from the authorized Speedway 95 tire representative. Buffing Allowed. Absolutely no tire softener. No chemicals may be used while mounting tires to rims. Any steel wheels 7" or 8" width to maintain 64" track width. Spacers allowed. Steel wheels required for all cars. Every wheel must have all wheel studs with required one (1) inch lug nuts.

All four wheels must have brakes in good condition.

Brakes must be OEM or OEM replacement type.

Cars may be converted to rear disk brakes using production model parts from any make or model.

Aftermarket brake pedal assemblies allowed

All tires must be the same size and style – no mixing tire brands or styles. Slicks will not be allowed.

Commonly available DOT treaded tire seven (7) inches maximum or Speedway 95 Official Track Tire

7" Wheels 50lbs break

Durometer testing will be done

Once Tires leave the truck ABSOLUTELY NO RETURNS

Absolutely no tire softener.

If tech suspects tire softener all tires will be dismounted and confiscated, and you will be disqualified. If protested a sample of the tire will be sent out for evaluation. The driver will be responsible for all costs of the testing if it is found positive.

Engine Specifications:

Twenty five hundred (2500) cc maximum.

.04 maximum overbore will be allowed.

Expanded/Racing oil pans will be allowed.

Dry sump pumps will not be allowed.

After market pulleys will be allowed.

Aftermarket heads will not be allowed.

Roller valve train parts allowed only if OEM parts. No aftermarket. Maximum 480 lift camshaft All pistons will be allowed.

Connecting rods must be steel.

Variable valve timing will be allowed.

Knife edging cranks will not be allowed.

Maximum compression ratio is 12:1

Engine must be of same manufacture as chassis. Honda for Honda, Toyota for Toyota etc. Engine must fit chassis with no modification of motor mounts.

Cooling/Battery/Exhaust:

Any radiator that fits under the hood is allowed.

Water and a cooling additive only. No antifreeze allowed.

Must have a minimum one (1) quart capacity sealed overflow container.

Car must not have fluid leaks of any kind.

No forced air induction (turbocharger/supercharger) allowed.

Battery may be relocated. Battery must be in a sealed battery box and properly secured.

Exhaust system with a muffler is required and must extend past the drivers compartment turning down toward the ground with the end of exhaust system under the race car, not out the side or the rear of the race car.

No anti-freeze allowed. Water and Keep Kool™ only. All cars must have an overflow jug in working order.

Ignition:

Any stock ignition or MSD will be allowed.

Fuel System:

A fuel cell is mandatory. Maximum twelve (12) gallon fuel cell, in a steel container with a check/rollover valve in the vent line; secured with two (2) straps front to rear and two (2) straps left to right. The minimum dimension of strap material is eighth (1/8) inch x one and one half (1½) inch.

All fuel filters must be of metal construction. No plastic, glass or other material will be allowed.

Any line containing a combustible material must be metal and routed outside of the driver compartment. Electric fuel pumps must be wired through a low oil pressure cut off switch.

Fuel Cell minimum ride height 8" from ground.

Transmission:

Transmission/Trans Axle must be stock production for make and model of car.

Car must have functional reverse gear.

Driveshaft must be steel and painted white.

Driveshaft loop required near the front of the driveshaft with material specification minimum of one-quarter (1/4) inch x two (2) inches. Rear driveshaft loop is recommended. Differential/Trans Axle may be locked.

Single disc clutch only. Any flywheel allowed.

Suspension:

Four (4) inch minimum ride height.

Track Width shall be sixty-four (64) inches maximum measuring from center to center.

All springs will be allowed.

Wedge adjusters will be allowed.

All sway bars will be allowed.

Shocks must be steel, smooth body, sealed, and non-adjustable/rebuild-able. Maximum cost \$100.00 per shock or strut. 50/50 shocks only. Shocks subject to tech testing at any time.

After market suspension parts will be allowed.

Traction Control will not be allowed.

No Bump stops allowed.

Windshield:

Windshield may be replaced with plastic or 1/8" Lexan™. Complete windshield opening must be covered.

No other glass allowed.

Brakes:

Four wheel brakes in proper working order are mandatory. Brake system must remain stock for make and model.

**Anything not covered by any rules for this division shall remain stock as made by the manufacturer.
Stock for year and make must apply in all cases.**

**THE TECH COMMITTEE RESERVES THE RIGHT TO ADD OR SUBTRACT WEIGHT AT
THEIR DISCRETION**

FOR EACH ITEM NOT MEETING SPEEDWAY 95 SPECIFICATIONS.

**THE TECH COMMITTEE MAY ADD OR SUBTRACT WEIGHT IN THE FAIRNESS OF
COMPETITION**

**Tech has the option to give last place points and loss of award money to a car/driver that has an
infraction to the rules that would not warrant a disqualification.**

**Any alteration to any part of the car not specifically covered in these rules must be cleared with the
technical inspector or it will be considered illegal. In other words, IF IT DOESN'T SAY YOU CAN
DO IT, YOU CAN'T.**

**Any specification not covered by these rules may be decided upon by tech management and made a
rule for the rest of the season.**

**Motorsports at all levels pose a risk to your wellbeing. Following the rules set for by Speedway 95 in
no way guarantees or suggests your safety.**

Tiger Rules

Chassis:

Any American branded, 1992 or older (93-2004 Mustangs may be used), V-8 front engine, rear wheel drive, full frame, or uni-body car originally mass produced with a minimum wheelbase of a 101". Engine and body must be of the same manufacturer as the chassis used. NO LEAF SPRING CARS. No Gen 4 Camaro.

Design:

Minimum four (4) point roll cage is required. In addition to the required four (4) point roll cage, the structure may extend to the front as far as the radiator support but not connect left and right strut towers on Uni-Body cars. The left side must have a minimum of four (4) horizontal bars with spacers. The right side must have a minimum of three (3) horizontal bars with spacers or an "X" and two (2) horizontal bars.

Door Plate:

A minimum of sixteen (16) gauge steel plate of sufficient length and width as to protect the driver must be welded to the roll cage.

Fire Walls:

The stock front and rear firewall must remain in the stock location and configuration. All holes must be covered with a minimum of 22-gauge steel and seal of the driver's compartment.

Frame:

Unibody Cars that have tubing structure integrated within chassis that extends more the 6" forward or backward of the 4-point cage structure will be subject to weight penalties as determined by technical inspector. No excessive tubing will be permitted.

Bumpers:

Stock front and rear bumpers may be used. Approved tube type front and rear bumpers may be used.

Weight:

3000 lbs., race ready. All cars being weighed for compliance with technical rules will be weighed with each wheel centered on a scale, the driver belted in the seat and sitting in the upright position with his/her helmet on or resting in his/her lap. **Any car with 3 wins will have 25 lbs added to the right side of the car, between the axels and will go up 25 lbs after every victory there after.**

Left side weight: Uni-body chassis 54% Full frame chassis 55%

Ballast Weight:

All weight must be between the center line of the rear axle and an imaginary line drawn through the front axle. All lead must be painted white with car # clearly marked. Lead needs to be mounted either with sandwich method threw floor with 1/2" bolts, or to frame.

K-Members:

NO altering of structure it MUST stay in stock form and location for make and model. No altering of mounts.

Fuel Cell:

16-gal max fuel cell and must be a steel container with a check/rollover valve in the vent line; secured with two (2) straps front to rear and two (2) straps left to right. The minimum dimension of strap material is eighth (1/8) inch x one and one half (1½) inch. Fuel cell may not be offset and must be a minimum of 12" above the track.

Height:

Minimum frame height from ground is 6" with the driver.

Interior:

Interior area of car must be completely STOCK and enclosed in respect to the engine compartment, track surface, tires, and rear (fuel cell) compartment. If holes in the floor need repair, they may be covered but must have original floor underneath or subject to a weight penalty.

Pipe:

Minimum roll cage tubing specification one and three quarter (1¾) inch x points zero nine five (.095) inch DOM or seamless tubing.

Rear Clip:

Rear frame should remain stock for make and model any reinforcing or replacing to the stock rear clip is subject to technical interpretation.

Seat:

Aluminum racing seat with right and left head supports. Seat must be mounted and attached securely to the cage. A support brace must be installed at the rear of the seat. The Drivers head should be below halo and clear of all roll bars while seated in the car.

Seat Belts:

Mounted with three eighths (3/8) inch bolts minimum Grade 8 washers and locking nuts. These must be attached at two (2) separate points on the frame, equal distance from the seat back and not less than twelve (12) inches apart. (See manufacturer instructions for details).

Steering Column:

A fabricated steering column can be used but must have a minimum of two (2) U-joints positioned so as not to allow the steering wheel to be pushed rearward in the event of a collision. Must have steering column stop. All steering wheels must have a center pad.

Welds:

All welds are to be of high quality and must surround the joint. Absolutely no butt welds or sharp edges anywhere.

Battery:

12-volt Battery mounted and covered in a secure fashion mandatory.

Wheelbase:

Stock for Make and Model plus or minus ½". Absolutely no modifications except as specified in this rule book.

Motors and Running Gear

All engines are to remain stock for year, make, and model The Max Cubic Inch 350 Stock or 305 Stock for General Motors and Ford Motors. The idea is to keep this a cost-effective division if it's not stock other than what's specified for changes it will be considered illegal.

Cylinder Heads:

Any 350 or 305-casting head with a stock 1.94 - 1.60 valve can be used that came on a motor for year make and model. NO VORTEC HEADS!!!

Block:

Stock block for make and model any tampering of block numbers will be an automatic disqualification.

Camshaft:

Flat tappet cams and with a max lift of .415 and .210 duration NO ROLLER CAMS

Pistons:

4 valve relief pistons only stock replacement may be used but must maintain 9.5-1 compression.

Intake:

Any stock cast iron 2-barrel intake that is made for a passenger vehicle. No bus, marine, or high-rise intakes allowed.

Exhaust:

Stock cast iron manifolds with 2 Thrush 17715 or equivalent suitcase 19-3/4" x 10" mufflers are mandatory. No Ram Horn or after Market

Radiator & Coolant:

Any radiator that fits in the stock location with a radiator cap. No closed systems overflow tanks are allowed. Any fan may be used. No antifreeze allowed.

Carburetors:

factory stock Rochester two-barrel carburetor bolted to a stock two-barrel cast iron intake may be used. Alternate of Holly 4412 non VDL must be spec from factory. **Adapter Plate 1937 Mister Gasket**

All other parts of the Motor must remain stock for Year, Make and Model

Driveline

Rearends: Stock housing for make and model 3.42-3.08 gear ratio with an open differential in any Uni-body car. Full frame cars can run a mini spool or locked rear with a max gear of 3.73-3.08. No ratcheting or torque-sensing devices or clutch packs allowed.

Driveshaft: Steel driveshaft (singular) only. The driveshaft must have a safety loop of 1/8 thick by 2" wide.

Transmissions: Stock automatic transmission for make and model. Turbo 350 are allowed. No Standard or power glides.

Body

Body must remain stock for year make and model with stock body lines and be steel. No T-tops or convertibles, any sunroofs must be covered with a steel sheet of no less than 22-gauge steel and be welded. All roof, quarter and windshield post inner reinforcement must remain in place.

Height: No part of the body (including side skirts and nose piece) may be closer than six (6) inches to the ground.

Spoiler: No rear spoilers of any kind.

Hood: Stock steel hoods only. The stock inner reinforcements may be removed but NO SHARP EDGES.

Bumper Covers: Aftermarket bumper covers allowed. The front must match year make and model.

Deck Lid: The rear deck lid must be hinged & pinned for viewing and safety access. No solid riveting.

Fenders: On Uni body cars the front inner plastic fender is the only thing that can be removed. The front inner fender from the fire wall to the radiator supports must remain untouched. Slotting of the upper strut mounts for camber is allowed but still must fit all camber and wheelbase rules.

Windows: A full windshield is required. The windshield must be a minimum of 1/8" thick polycarbonate material such as Lexan. A minimum of two windshield braces are required. No rear windows allowed. Window installations are subject to the Tech Inspector's approval.

Suspension

All front suspension parts are to remain stock for year make and model no interchanging from year to year. Rubber bushings or neoprene replacements are allowed.

Springs: Same wire size, height, and diameter left to right. Following these perimeters rates should be equivalent.

Sway Bars: Stock for year make and model in the original location using stock mounts. Max sway bar size is 32MM.

Sway bar Links: Stock links with all stock hardware must be maintained on the left and right. The left side can be adjustable as long as stock hardware and bolt lengths match. Spacer can be shortened up to 1" on left side.

Torque Arm-Third Member: Stock dimensions in a stock location mounted in stock rubber nonadjustable 6-1/2" or less from the transmission tunnel.

Trailing Arms: Stock for year make and model in stock location.

Camber: Maximum of 1-1/2" of positive camber on all Uni-Body car 2" on all full frame car on the right front. Maximum camber on the LF is 3/4".

Tires and Wheels

Tires: American Racer 7" 225 SH or H track tire, EITHER SIDE with any combination are the only tires approved. Absolutely no chemically treating of tires at any time.

Bleeders: Absolutely no tire bleeding devices allowed

Wheels: Steel 7"x15". Unibody cars Max track width 64 1/2" Full frame Max track width 65"

Brakes: All cars must have four working brakes on the car. All brakes including the pedal assembly must remain stock.

Transponders & RACEeivers: Transponders & RACEeivers are mandatory. Transponders shall be mounted outside of the car on 12'6" from the front bumper.

**After 3 wins 25lbs will be added to the right side of the car and
will go up 25lbs after every victory thereafter.**

Speedway95 officials may assess weight penalties for race equipment deemed not in compliance with the rules. Race equipment will not be considered as approved by reason of having passed through inspection at any time – or any number of times – unobserved or undetected. Any race equipment which does not conform to specifications contained in the rule's listings, or that is not otherwise approved by the Speedway 95 may not be used in competition.

CageRunner Rules

Vehicles must be street legal for make and model except for modifications made for racing allowed by these rules. Passenger vehicles only. No convertibles, Full size vans, Full Size trucks, Chrysler Imperials, station wagon or full frame Oldsmobile Toronados. No leaf spring vehicles. No ALL wheel drive cars. **No turbochargers.**

Vehicles must be street legal for make and model, except for modifications made for racing allowed by these rules.

Clothing:

All drivers in all divisions must wear an approved long sleeve fire retardant racing suit at all times when the vehicle is on the racing surface. This includes test and tune sessions. Racing suits must be free of rips, tears and holes, and have a date of manufacture of not more than four years old, to be checked a few times a year by the safety committee. Fireproof racing suits and racing gloves are MANDATORY.

Exterior:

DOORS ARE REQUIRED TO BE WELDED SHUT FROM THE EXTERIOR. EXTERIOR PIPING REQUIRED AS SPECIFIED ON DIAGRAM ON PAGE 48. ALL HUBCAPS MUST BE REMOVED

**VEHICLE MUST START THE EVENT WITH ALL BODY PARTS
MAY REMOVE FLAMMABLE MATERIALS, BUT DASH MUST STAY INTACT**

Windshield: Front windshield must be in place when racing. Windshield must be stock glass or 1/8" Lexan™. All other glass i.e. headlights and marker lights must be removed from the vehicle before it will be allowed to compete. Window Nets Mandatory

FIRE EXTINGUISHERS: OPERATIVE 3# FIRE EXTINGUISHER WITH GAUGE MOUNTED IN A METAL BRACKET WITHIN REACH OF THE DRIVER IS REQUIRED.

Tires and Wheels:

1" wheel tilt on right front. Wheel size must be stock for vehicle. All wheels on car must have the same offset, no mixing and matching offsets. All wheel studs and lug nuts must be in place. No directional tires. No recaps. All tires on the vehicle must be the stock for the Make/Model of the car and same size/series tires all the way around, 2-4 ply only. No liquid filled. No angle tire buffing allowed straight buffing allowed. Treadwear rule 380 minimum.

Suspension:

No cutting of suspension. Slotting of the upper strut towers allowed – straight in only. Stock unaltered.

Seat: All stock seats must be fastened in a stationary, upright position. Racing seat may be used with approved bracing.

Seat Belts: Racing harnesses Required and must be in good condition

Helmet: All drivers must wear a DOT approved helmet

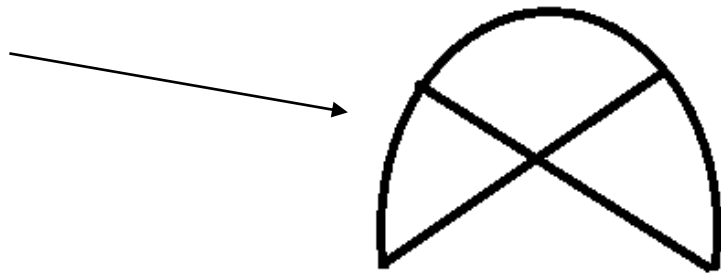
Must keep Stock Exhaust

Battery must be in stock location and securely fastened.

Roll Cage:

An X-Hoop with two door bars or a four point roll is MANDATORY in Caged Runner Division see page 54 & 55. A roll cage must consist of four uprights joined together at all four corners at the top. Tubing must be at least 1 ½" Round O.D. and minimum .090 wall thickness. All roll cages must be professionally welded. An "X" must be placed between the rear uprights, or a horizontal bar may be used to join the two rear uprights at the bottom, with a diagonal bar joining the top and bottom horizontal bars.

X-Hoop



minimum frame

Frame:

height 6" with driver in car 6" with driver in car

Roll Cages:

1. Four bars running between front and rear uprights are required on the driver's side. These bars must be padded.
2. A minimum of three horizontal bars or an "x" is required on the passenger side of the vehicle.
3. A horizontal bar between the two front uprights just above or below the dashboard is required.
4. Forward or rearward bars attached to the main uprights are NOT allowed.
5. The roll cage may not reinforce the body or frame or alter the geometry of the vehicle.
6. Roll bars may NOT extend through firewalls.
7. All vehicles with frames must have the roll cage welded directly to the frame NOT the floorboards.
8. All vehicles without frames must use the sandwich method shown in the roll cage diagram to secure the roll cage to the vehicle.
9. May have rearward bars no further than firewall or center of strut towers

All cars must have tow hooks on front and rear.

**After 3 wins 25lbs will be added to the right side of the car and
will go up 25lbs after every victory thereafter.**

Motorsports at all levels pose a risk to your wellbeing. Following the rules set for by Speedway 95 in no way guarantees or suggests your safety.

All drivers in all divisions must wear an approved long sleeve fire retardant racing suit and racing gloves at all times when the vehicle is on the racing surface

Modified Enduro Rules

Vehicles must be street legal for make and model except for modifications made for racing allowed by these rules. Passenger car vehicles only. No convertibles, vans or trucks. No leaf spring vehicles. No All Wheel Drives. No superchargers.

Engines:

4 or 6-cylinder engines only. Any stock engine and HP for year, make and model of car.

Wheel & Tires:

Wheel size must be stock for vehicle, make, model and year. Maximum 7" wheel. All wheel studs and lug nuts must be in place. NO RACING WHEELS. Stock steel/or aluminum wheels only. All wheels on the vehicle must be the same size and made of the same material, Aftermarket steel wheels allowed, must be same width and offset as stock. Tires may vary by one size side to side, for example: 175 on left and 185 on the right, but both tires on each side must be the same size. No directional tires. No recaps, 2-4 ply only. No liquid filled. AIR ONLY. Maximum allowable tread width of tires is 7". Tires must have minimum 380 tread wear.

Volkswagen wheels allowed

Buffing is allowed. No tire softener allowed.

Bumpers:

Bumpers must be at stock height for make and model of vehicle. A bumper may be fabricated from thin wall square tubing, but must mount to stock mounts or to round tubing slipped over and bolted to stock mounts. All vehicles must have bumper covers to start the race. Rear bumper/bumper covers should be strapped to body or frame to eliminate dragging. Modified Enduro can run after market front and rear bumper covers, but must have stock bumper or replacement as noted above under it. Bumper covers must match make of car. A protection loop in front of the radiator may be added to the bumper, with one brace on each side attaching to the frame just in front of the radiator.

Weight:

Left side weight rule **54%** of total weight maximum. No adding weight. All cars being weighed for compliance with technical rules will be weighed with each wheel centered on a scale, the driver belted in the seat and sitting in the upright position with his/her helmet on or resting in his/her lap. **Any car with 3 wins will have 25 lbs added to the right side of the car, between the axels and will go up 25 lbs after every victory there after.**

Windshield:

Front windshield must be in place when racing. Windshield must be stock glass or 1/8" Lexan™. All other glass i.e. headlights and marker lights must be removed from the vehicle before it will be allowed to compete. Rear side windows may be replaced with 1/8" Lexan or sheet metal.

Body/Frame:

5" ride height (any part of the body or frame) w/driver in car. All doors must be welded shut. No body alterations allowed EXCEPT AS NOTED IN THIS SECTION. All floor carpets and seats must be removed.

Roll bar or four-point cage with door bars is **REQUIRED**. Dashboard may be removed for installation of roll cage only. Steering column must remain stock. Back of rear seat **MUST** be replaced with sheet metal. Fenders may be trimmed one (1) inch for tire clearance. No gutting of the interior metal except for installation of roll cage. No gutting of hoods, trunks or inner fenders. Hood and trunk lids must be on hinges. Tech will determine if too much has been removed and may require re-installation. Interior door panels may be removed for door bar and safety plate installation only. If an interior door panel is removed, there must be bars in that area that are attached to the roll cage. No fiberglass body panels allowed. If driver's doorplate is installed, it must be padded. .22 gauge Steel may be used to replace doors, as long as door appearance remains stock and stock door contours are followed. Lexan Rub Rails Allowed

Seat: Aluminum racing seat required

Seat Belts: Racing harness must be used and must be approved by Safety Team.

Helmet: All drivers must wear a DOT approved helmet.

Racing Suit: Fireproof racing suits and racing gloves are **MANDATORY**.

Battery: Batteries **MUST** be placed in rear seat area in a box completely covering the battery.

Brakes:

All vehicles must have four-wheel brakes in proper working order, **NO EXCEPTIONS**.

Suspension:

2" maximum tire tilt on left and right front allowed. One spring rubber allowed per car. No rear wheel tilt. No cambered rear ends. No lowering of the body on the frame. Stock springs may be cut to achieve frame height. Springs must be same size wire. Sway-bar arrangement must be the same on both sides - stock length, with stock parts. No adjustable shocks, no racing springs, no adjustable cups. Stock style upper control arm replacements may be used. No angle type or offset type can be used. Ball joints must be welded to control arm. No spacers on top of struts. All stock bump stops must be removed

Gears/Transmission:

All running gears must remain completely stock for model and generation of vehicle. No 2 speed automatics. Absolutely No Lockers Allowed.

Rear End: Must be stock for year, make and model. Mufflers are mandatory.

Fire Extinguishers: All vehicles must have an operative working 3# fire Extinguisher with gauge and mounted in a metal bracket within reach of the driver.

Window Nets: Window nets are mandatory with a working seat belt type hook.

RADIATOR: NO ANTI-FREEZE ALLOWED. KEEP KOOL™ OR WATER ONLY. VEHICLES MUST HAVE AN OVERFLOW JUG IN WORKING ORDER.

Tow Hooks: ALL CARS MUST HAVE FRONT AND REAR TOW HOOKS OR CHAINS EASILY ACCESSIBLE TO WRECKER CREWS. No Tow hooks means no racing.

The track and its tow crews will have no responsibility whatsoever for damage resulting from towing a disabled vehicle from the racing area.

Fuel Tanks: Fuel cells and fuel tanks must be securely mounted in the trunk area. No cutting of trunk floor to lower fuel cell/tank in car. Boat Tanks must be in a canister of no less than .22-gauge steel. May have a loop to protect fuel cell, plated and welded to trunk floor.

A four-point roll cage is mandatory as explained/diagramed on page 64 & 65 Anything not covered by any rules for this division shall remain stock as made by the manufacturer. Stock for year, make or generation” of car must apply in all cases.

THE TECH COMMITTEE RESERVES THE RIGHT TO ADD OR SUBTRACT WEIGHT OR TO RESTRICT AIR FLOW TO THE ENGINE AT THEIR DISCRETION

Tech has the option to give last place points and loss of award money to a car/driver that has an infraction to the rules that would not warrant a disqualification.

Any alteration to any part of the car not specifically covered in these rules must be cleared with the technical inspector or it will be considered illegal. In other words, IF IT DOESN'T SAY YOU CAN DO IT, YOU CAN'T.

Any specification not covered by these rules may be decided upon by tech/ management, and made a rule for the rest of the season.

Disclaimer:

Motorsports at all levels pose a risk to your wellbeing. Following the rules set for by Speedway 95 in no way guarantees or suggests your safety.

RoadRunner Rules

RoadRunners Vehicles must be street legal for make and model except for modifications made for racing allowed by these rules.

Passenger vehicles only.

No convertibles, Full size vans, Full Size trucks, Chrysler Imperials, station wagons, full frame Oldsmobile or Toronados.

No leaf spring vehicles.

No ALL wheel drive cars.

No turbochargers, No superchargers.

Absolutely No Lockers Allowed.

Weight:

Any car with 3 wins will have 25 lbs added to the right side of the car, between the axels and will go up 25 lbs after every victory there after.

Exterior:

Doors are required to be Welded Shut from the Exterior. Exterior Piping must be bolted

One bar must extend from the front fender to the rear door, the top bar must be lower than the top of the fender. the bottom bar must be separated from the top bar. No exception, no bolting, no racing.

2 – 2” steel pipes or tubes on driver’s side required - passenger side door optional.

All hubcaps must be removed.

Vehicle must start the event with all body parts Must have complete dash minus minimal for cage installation.

Windshield:

Front windshield must be in place when racing.

Windshield must be stock glass or 1/8” Lexan™.

All other glass i.e. headlights and marker lights must be removed from the vehicle before it will be allowed to compete.

No lights of any kinds will be allowed at any time.

Window Nets Mandatory

Fire Extinguishers:

Operative 3# fire extinguisher with a gauge mounted in a metal bracket within reach of the driver is required.

Tires and Wheels:

Wheel size must be stock for the vehicle either steel or aluminum, no mixing.

All wheel studs and lug nuts must be in place.

No recaps

All tires on the vehicle must be the stock for the Make/Model of the car and same size/series tires all the way around, 2-4 ply only.

No liquid filled.

All hubcaps must be removed.

stock size tires for year, make and model

380 Treadwear minimum

No Buffing or cutting of tires allowed

Suspension

Stock bump stops allowed on all 4 shocks or none

½" camber on right front ONLY

Seat:

All stock seats must be fastened in a stationary, upright position.

Racing seats may be used with approved bracing.

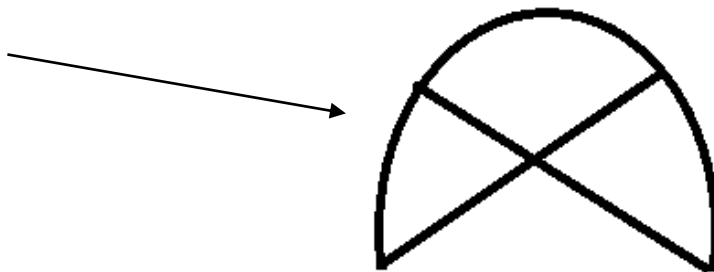
A hoop may be installed in the car behind the driver to protect the driver from roof collapse.

Roll Cage is permitted

Gutting of fabric and plastic is allowed with exception of dash.

Racing seat and 4 point cage recommended, not required.

X-Hoop



Seat Belts:

Racing harnesses **MUST** be used and must be in good condition

Helmet:

All drivers must wear a DOT approved helmet

Radiator:

No Antifreeze allowed. KEEP KOOL™ or water only.

Vehicles must have an overflow jug in working order.

Tow Hooks

All vehicles must have front and rear tow hooks made of 1/2" rolled steel and installed as a vertical loop.

All cars must have tow hooks on front and rear.

No Tow hooks means no racing. No Chains & No Straps around the bumper

No altering of suspension.

Battery must be in stock location and securely fastened.

All cars must have tow hooks on front and rear. No Tow hooks means no racing.

The track and its tow crews will have no responsibility whatsoever for damage resulting from towing a disabled vehicle from the racing area.

Any cars left at the race track for more than 7 days after racing for that division. Will be removed at the car owner's expense.

Flag Rules

Flag rules will follow all other division flags except for events that have over 30 cars. If there are more than 30 cars racing the top 20 cars will be lined up according to regular racing line ups and the rest will scramble for their positions.

Scoring

A MyLaps compatible transponder will be required to be scored, any car that does not have a transponder for the duration of the event will not be scored. Transponders will be available at the track for rental on a first come first serve basis.

Interior of Car

Stock dash required in original position. Rest of interior must be removed.

Tech

Cars will be teched to follow these rules. IF your car does not meet these rules you will be disqualified

Cars must be 100% Stock, no alterations other than set by these rules.

All vehicles must have front and rear tow hooks made of 1/2" rolled steel and installed as a vertical loop.

**All cars must have tow hooks on front and rear.
No Tow hooks means no racing.**

No Chains & No Straps around the bumper.

The track and its tow crews will have no responsibility whatsoever for damage resulting from towing a disabled vehicle from the racing area.

Motorsports at all levels pose a risk to your wellbeing. Following the rules set for by Speedway 95 in no way guarantees or suggests your safety.

Any cars left at the race track for more than 7 days after racing for that division. Will be removed at the car owner's expense.

Truck Rules

Bodies:

TRUCKS MUST BE 1973 OR NEWER, FULL-SIZE RATED, WITH A MINIMUM 115" WHEELBASE. TRUCKS MUST HAVE A FULL STOCK STEEL BED WITH A TAILGATE IN PLACE AND WELDED SHUT. NO FLAT BEDS PERMITTED. BED COVERS ARE PERMITTED, WITH 18" X 18" HATCH. 6" SPOILER ALLOWED. PANELS MAY BE FABRICATED FROM .22 GAUGE STEEL. REAR BODY MUST HAVE A COVER.

Weight:

3,500 LBS MINIMUM LEFT SIDE WEIGHT RULE 53.5% OF TOTAL WEIGHT. ANY WEIGHT ADDED TO VEHICLES MUST BE LEAD ONLY AND PAINTED WHITE AND SECURELY FASTENED.

Tires and Wheels:

Wheels are limited to a maximum width of 8 inches

wide. a 8" Goodyear slick take off tire instead of the street stock tire. These tires will be available at Speedway for \$35 each. This was done to make the division more affordable to the drivers! Tires must be purchased from the authorized Speedway 95 tire representative. **If caught with/using tire softener driver will lose all accumulated points to date. Once Tires leave the truck ABSOLUTLY NO RETURNS** 1" lug nuts required on right side of truck. Four-wheel drive trucks are permitted, but front drive shaft must be removed. Dual wheel chassis not allowed.

Engine:

NO 2 VALVE RELIEF PISTONS. ENGINE MUST BE 6 OR 8 CYLINDERS. MUST PULL 15 LBS OF VACUUM AT 500 RPM'S. STOCK CAST IRON 2-BARREL INTAKE MANIFOLD. NO ADJUSTABLE CARB SPACER. STEEL EXHAUST MANIFOLDS OR HEADERS. STOCK 2-BARREL CARBURETOR OR FUEL INJECTION, WHICHEVER COMES STOCK. RACING OIL PANS & RACING VALVE COVERS PERMITTED. COMPRESSION RATIO MAXIMUM 9.7:1. ENGINES MUST BE MOUNTED IN STOCK POSITION NO SET BACK ALLOWED.

Fuel Cell:

STOCK FUEL TANK MUST BE REMOVED. TRUCKS MUST HAVE A FUEL CELL NO LARGER THAN 16-GALLON CAPACITY MOUNTED IN THE TRUCK BED. THE FRONT EDGE OF THE TANK/FUEL CELL MUST BE LOCATED DIRECTLY OVER THE CENTERLINE OF THE TRUCK AND TRUCK BED.

Frame:

OUTRIGGERS ALLOWED ON BOTH SIDES OF THE TRUCK TO SUPPORT THE 4-POINT CAGE ONLY. FUEL CELLS MUST BE LOCATED UNDER A BOXED STRUCTURE OF MINIMUM .22-GAUGE STEEL, AND MUST BE FASTENED TO THE TRUCK BED IN A MANNER PREVENTING THE CELL FROM BECOMING DISLODGED UPON A SUDDEN HARD IMPACT IN COMPETITION. ALL TRUCKS MUST HAVE AN APPROVED ROLL CAGE IN THE CAB. REFER TO ROLL CAGE SECTION FOR GUIDELINES. CAN HAVE REAR BARS EXTENDING NO FURTHER BACK THAN FRONT OF FUEL CELL. FRONT HOOP ALLOWED, CAN ONLY GO BACK TO FRONT A FRAME **CAN BE TIED INTO MAIN CAGE. MINIMUM RIDE HEIGHT 8" MEASURED AT FRONT CAB MOUNT NEXT TO FRAME AND 11" MEASURED AT FRAME IN FRONT OF REAR SPRING HANGER. NO MORE THAN 1" VARIANCE LEFT TO RIGHT IN RIDE HEIGHT.**

Steering:

RACING STEERING COLUMNS ARE ALSO PERMITTED.

Rear Ends:

**LOCKED REAR ENDS AND POSSI-TRACS ALLOWED. 3.73 MAXIMUM GEAR RATIO ALLOWED.
MAX. TRACK WIDTH 66 ½"**

Transmissions:

**3 SPEED STANDARD OR 3 OR 4 SPEED AUTOMATICS. NO TWO (2)
SPEED AUTOMATICS.**

Drive Shaft:

**DRIVE SHAFT SAFETY LOOPS REQUIRED FOR EACH DRIVE SHAFT SECTION. LOOPS AND DRIVE
SHAFT MUST BE PAINTED WHITE.**

Radiator:

**FULL FRONT HOOP ALLOWED. NO ANTI-FREEZE ALLOWED. KEEP KOOL™ OR WATER ONLY.
TRUCKS MUST HAVE AN OVERFLOW JUG IN WORKING ORDER.**

NO FRONT SEAT SETBACK IN EXTENDED CAB TRUCKS.

**FIRE EXTINGUISHERS: OPERATIVE 3# FIRE EXTINGUISHER WITH GAUGE MOUNTED IN A METAL
BRACKET WITHIN REACH OF THE DRIVER IS REQUIRED.**

MUFFLERS ARE MANDATORY.

Suspension:

**STOCK SPRING IN STOCK AMOUNTS. "OLD STYLE" SWAY BAR ALLOWED TO SHIM EQUALLY
ON BOTH SIDES. STOCK NON-ADJUSTABLE SHOCK ABSORBERS ONLY. NO RACING SHOCKS
ALLOWED. NO JACKING BOLT. NO ADJUSTABLE CUP. NO RACING SPRINGS. 1 SHOCK PER
WHEEL**

Racing Stars of Tomorrow Rules

Wednesday Nights Ages 10 – 15*

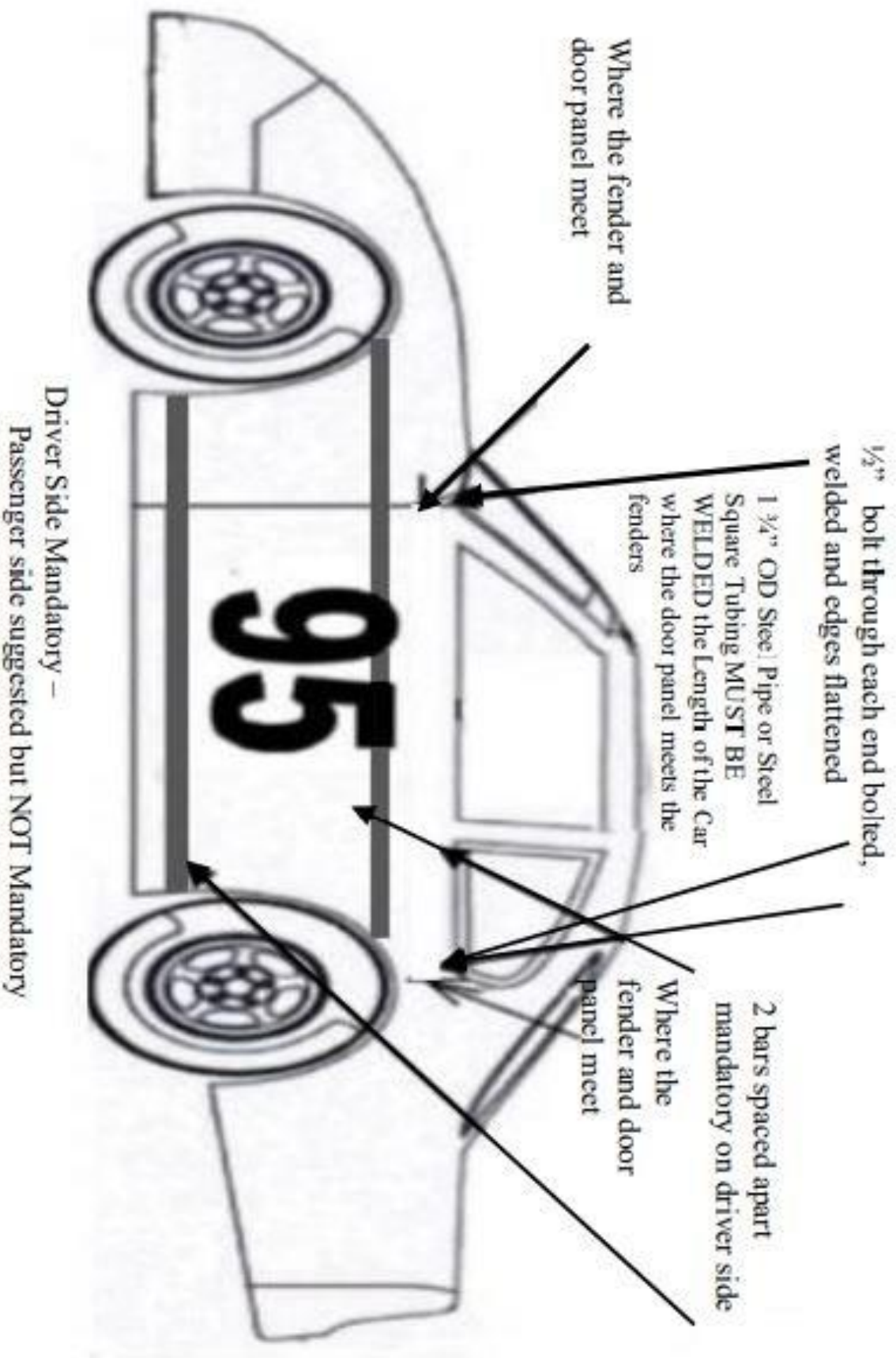
*Must not be more than 15 years of age throughout the racing season

No driving participation allowed in any racing division other than Racing Stars of Tomorrow, either on Wednesday or Saturday race meets at Speedway 95.

CERTIFIED copy of Birth Certificate AND current notarized minor release form to be on file with Speedway 95 Promotions, Inc. for the whole racing season. **ALL minor drivers must be accompanied in the pit area by a legal parent.**

1. 4 Cylinder Only
2. Fuel Cell REQUIRED
3. Saturday Safety Rules apply
4. Saturday Racing Rules apply
5. Four point roll cage REQUIRED (see page 64 & 65)
6. Full Racing Suit REQUIRED
7. Helmet REQUIRED
8. **Racing gloves are MANDATORY**
9. Aluminum Seats ONLY
10. Right side head rest REQUIRED. Left Side OPTIONAL
11. Neck **Restraint** REQUIRED
12. Racing Seat belts REQUIRED (five point harness) **within date range**
13. Weight Rule: **54%** left side weight, can add weight if needed not to exceed 54% left side weight.
14. No body alterations allowed
15. Absolutely No Lockers Allowed.
16. **All other rules will follow Modified Enduro Rules**
All cars must have tow hooks on front and rear.

Stars of Tomorrow will not be allowed to use 2-way radios, or any communication device while racing in the Stars Of Tomorrow. Stars of Tomorrow Drivers will be required to use a RACEceiver on Channel 454.000.



Roll Cages

1. A four-point roll cage is mandatory in MODIFIED ENDURO and RACING STARS of TOMORROW Divisions. If roll cage is used it must be approved by safety. A roll cage must consist of four uprights joined together at all four corners at the top. Tubing must be at least 1 ½" Round O.D. and minimum .090 wall thickness. All roll cages must be professionally welded.
2. An "X" must be placed between the rear uprights, or a horizontal bar may be used to join the two rear uprights at the bottom, with a diagonal bar joining the top and bottom horizontal bars.
3. Four bars running between front and rear uprights are required on the driver's side. These bars must be padded.
4. A minimum of three horizontal bars or an "x" is required on the passenger side of the vehicle.
5. A horizontal bar between the two front uprights just above or below the dashboard is required.
6. Forward or rearward bars attached to the main uprights are NOT allowed.
7. The roll cage may not reinforce the body or frame or alter the geometry of the vehicle.
8. Roll bars may NOT extend through firewalls.
9. All vehicles with frames must have the roll cage welded directly to the frame NOT the floorboards.
10. All vehicles without frames must use the sandwich method shown in the roll cage diagram to secure the roll cage to the vehicle.
11. May have rearward bars no further than firewall or center of strut towers

SUGGESTED ROLL CAGE DESIGN

